



HILLINGDON
LONDON

A

VIRTUAL Major Applications Planning Sub-Committee (HS2)

To Councillors on the Committee

Councillor Steve Tuckwell (Chairman)
Councillor Edward Lavery (Vice-Chairman)
Councillor Nicola Brightman
Councillor Becky Haggar
Councillor Janet Duncan (Opposition Lead)
Councillor John Oswell
Councillor John Morgan

Date: THURSDAY, 29 OCTOBER 2020

Time: 6.00 PM

Location: VIRTUAL MEETING

Watch Live Watch a live broadcast of this meeting on the Council's YouTube channel: [Hillingdon London](#)

Important Meeting Advice: Following UK Government advice to avoid all but essential travel and to practice social distancing, the usual physical public speaking rights at Planning Committee will be suspended temporarily during the coronavirus pandemic. Instead, written representations will be considered as part of Hillingdon's established Petitions Scheme.

Published: 21 October 2020

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Agenda

CHAIRMAN'S ANNOUNCEMENTS

- 1 Apologies for Absence
- 2 Declarations of Interest in matters coming before this meeting
- 3 To sign and receive the minutes of the previous meeting dated 24 June 2020 1 - 4
- 4 Matters that have been notified in advance or urgent
- 5 To confirm that the items marked in Part 1 will be considered in public and those items marked in Part 2 will be heard in private

PART I - Members, Public and Press

Items are normally marked in the order that they will be considered, though the Chairman may vary this. The name of the local ward area is also given in addition to the address of the premises or land concerned.

Major Applications without Speaking Rights

- 6 ECOLOGICAL MITIGATION SITE, HARVIL ROAD - 73195/APP/2020/1946

Request for approval of Plans and Specifications under condition imposed by paragraphs 2 and/or 3 to Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017 for works comprising the erection of a bat house.

Pages 5-18 and 36-42

Recommendation: Approval

- 7 BETWEEN RIVER COLNE & G.U.CANAL NORTH OF DENHAM LOCK - 42471/APP/2020/2631

Request for approval of 'bringing scheduled works and depots into use' (Paragraph 9) under Condition imposed by Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017 relating to the diversion of the National Grid ZC overhead power line in the Colne Valley .(Schedule 1 Work No. 2/4B) -a temporary bridge over the River Colne).

Pages 19-34 and 43-52

Recommendation: Approval

PART I - Plans for Major Applications Planning Sub-Committee – pages 35 - 52

Agenda Item 3

Minutes

MAJOR APPLICATIONS PLANNING SUB-COMMITTEE (HS2)

24 June 2020



Meeting held at VIRTUAL - Live on the Council's YouTube channel: Hillingdon London

	<p>Committee Members Present: Councillors Steve Tuckwell (Chairman) Eddie Lavery (Vice-Chairman) Cllr Alan Chapman Becky Haggar Janet Duncan John Oswell John Morgan</p> <p>LBH Officers Present: Raj Alagh, Borough Solicitor James Rodger, Head of Planning, Transportation and Regeneration Luke Taylor, Democratic Services Officer Ian Thynne, Planning Soecialists Team Leader</p>
11.	<p>APOLOGIES FOR ABSENCE (<i>Agenda Item 1</i>)</p> <p>Apologies for absence were received from Cllr Brightman, with Cllr Chapman substituting.</p>
12.	<p>DECLARATIONS OF INTEREST IN MATTERS COMING BEFORE THIS MEETING (<i>Agenda Item 2</i>)</p> <p>There were no declarations of interest.</p>
13.	<p>TO SIGN AND RECEIVE THE MINUTES OF THE PREVIOUS MEETING (<i>Agenda Item 3</i>)</p> <p>RESOLVED: That the minutes of the meetings held on 14 May 2020 and 20 May 2020 be agreed as a correct record.</p>
14.	<p>MATTERS THAT HAVE BEEN NOTIFIED IN ADVANCE OR URGENT (<i>Agenda Item 4</i>)</p> <p>None.</p>
15.	<p>TO CONFIRM THAT THE ITEMS MARKED IN PART 1 WILL BE CONSIDERED IN PUBLIC AND THOSE ITEMS MARKED IN PART 2 WILL BE HEARD IN PRIVATE (<i>Agenda Item 5</i>)</p> <p>It was confirmed that all items were marked Part I and would be considered in public.</p>

16. **LAND AT DEWS LANE, DEWS LANE, HAREFIELD - 75435/APP/2020/1057**
(Agenda Item 6)

Request for approval of plans and specifications under condition imposed by Schedule 17 of the High Speed Rail (London – West Midlands) Act 2017 relating to the creation of new Dewes Lane alignment (schedule work 2/2) running parallel to existing Dewes Lane, creation of earthworks, erection of fencing, gabion wall and headwalls and creation of earthworks to facilitate a drainage pond.

Officers introduced the application, and the Borough Solicitor stated that there were no statutory grounds to refuse or add conditions to the application. Members heard that two informatives were attached to the application, but there were no legal impediments to the application being approved.

Responding to questioning from the Committee, the Planning Specialists Team Leader confirmed that landscaping would be considered in a later application, but tree planting had already taken place to the east of the site. Councillors heard that more trees would be planted than would be lost, so there should be no net loss in trees, while the feeder station would also be covered by planting.

Members agreed that the application was straightforward, as there were no statutory grounds under the HS2 Act to object to the application. However, the Committee expressed a desire to receive the whole picture for the site.

The Committee noted that there was no alternative to moving the application, and the officer's recommendation was moved, seconded, and unanimously agreed at a vote.

RESOLVED: That the application be approved, subject to the following informatives:

- 1. The proposed development includes measures relating to a number of different protected species. The proposals must be designed in accordance with any relevant protected species licence and ecological standards for the project; and,**
- 2. The nominated undertaker must work with the Council to identify the impacts of the proposals on the surface water drainage regime in the area, including impacts on the New Years Green Bourne and adopted highway. This work must be undertaken on a strategic basis, and factoring in all the relevant parts of this project.**

17. **NATIONAL GRID ZC DIVERSION - CONSTRUCTION SITE & COMPOUND OFF HARVIL ROAD, HAREFIELD - 74320/APP/2020/20** (Agenda Item 7)

Request for approval of Lorry Routes under condition imposed by Schedule 17 of the High Speed Rail (London – West Midlands) Act 2017 relating to the diversion of the National Grid ZC overhead power line in Colne Valley.

Officers introduced the application and noted that this application was deferred from a previous meeting as late information was received, but the information has now been submitted and is supported by the Council's Highways team.

The Borough Solicitor noted that the report was deferred to allow for more detail on access, particularly the access to the northern site, from HS2 Ltd and that this had now been provided. The Committee heard that the Borough Solicitor was of the view that

the application agreed a satisfactory balance between allowing works to take place and protecting the integrity of the Borough's highways. The Borough Solicitor confirmed that the approval was subject to an informative ensuring that appropriate signage be installed north and south of the access route, warning of the potential for slow-moving vehicles in the road. This informative also stated that there must be no queuing traffic on Harvil Road, and the applicant must monitor the turning and provide the Council with updates regarding its safety.

Members discussed the benefits of allowing Heavy Goods Vehicles (HGVs) to turn right to leave the site, and whether this would cause queues from Newyear's Green Lane to access amenity sites. While the Committee noted these concerns, Councillors agreed that sightlines were good on Harvil Road, and turning left out of the access could be more problematic due to housing and shops in the locality.

Officers confirmed that the application was based upon 50 lorries operating at the site, but further details were required from HS2 Ltd, and these would include a Local Transport Management Plan.

The Committee agreed that, in light of legal advice, there were no grounds to change the application. As such, the officer's recommendation was moved and seconded. Upon being put to a vote, the recommendation was unanimously approved.

RESOLVED: That the application be approved, subject to the following informative:

- 1. The applicant must ensure that appropriate signage is installed north and south of the access road providing clear notice of the potential for slow moving vehicles in the road. The applicant must also monitor the use of turning and provide the Council, as the highways authority, necessary updates with regards to safety. The Council reserves the right to seek alternative arrangements if there is evidence the access is being used in an unsafe manner.**

There must be no queuing traffic on the Harvil Road. All vehicles waiting to be moved to the main parts of the worksite must not be held on Harvil Road; the access road or alternative locations within the site must provide the holding areas for waiting vehicles.

The meeting, which commenced at 6.04 pm, closed at 6.38 pm.

These are the minutes of the above meeting. For more information on any of the resolutions please contact Luke Taylor on democratic@hillington.gov.uk. Circulation of these minutes is to Councillors, Officers, the Press and Members of the Public.

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Agenda Item 6

Item No. Report of the Head of Planning and Enforcement

Address: ECOLOGICAL MITIGATION SITE HARVIL ROAD HAREFIELD

Development: Request for approval of Plans and Specifications under condition imposed by paragraphs 2 and/or 3 to Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017 for works comprising the erection of a bat house.

LBH Ref Nos: 73195/APP/2020/1946

Drawing Nos:	Date of Plans:
1EW03-FUS-EV-DGA-CS01_CL01-014303	26-06-2020
Harvil Road Bat House Cover Letter.	26-06-2020
1EW03-FUS-PL-DGA-CS01_CL01-000005	26-06-2020
Harvil Road Bat House Written Statement	29-06-2020
Colne Valley Key Environmentally Sensitive Worksite Management Plan	29-06-2020
1EW03-FUS-EV-DGA-CS01_CL01-014302	26-06-2020
1EW03-FUS-EV-DGA-CS01_CL01-014304	26-06-2020
Harvil Road Bat House Proforma	29-06-2020
1EW03-FUS-EV-DGA-CS01_CL01-014301	26-06-2020
1EW03-AEC-PL-DSK-CS01_CL01-013300	05-10-2020

Drawing Nos:	Date of Amended Plans:
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Date Application Valid: 26th June 2020

1. SUMMARY

This application comprises a Plans and Specifications submission under Schedule 17 of the HighSpeed Rail (London-West Midlands) Act 2017 (The Act), in relation to permanent above ground features associated with the erection of a bat house on the Harvil Road ecological habitat creation site.

The application is the the latest HS2 Schedule 17 planning submission that has been deposited with the Council. These Schedule 17 planning submissions can best be likened to the submission of reserved matters, where outline planning consent has already been granted. However, the role of the Planning Authority is heavily restricted as to what can and cannot form the basis of a decision.

There is no statutory obligation to consult with neighbours. However, Natural England and Historic

England (GLAAS) are statutory consultees for this proposal and have raised no objections.

Officers are of the opinion that the proposals would not have a detrimental impact on a site of ecological value (i.e. a designated site) or an archaeological area of importance. Officers acknowledge that the bat house will provide a valuable habitat in this area and will assist in the long term conservation of important species.

Historic England (GLAAS) considers that the proposal will not have a significant effect on heritage assets of archaeological interest and is satisfied that the question of amending the design to preserve an archaeological site does not arise. Officers are of the opinion that the proposals would not have a detrimental impact on an archaeological area of importance.

The design of the bat house is broadly acceptable and it is considered that there are no reasonably practicable measures which need to be taken for the purpose of mitigating the effect of the work or its operation in terms of its impact on the local environment / local amenity, in accordance with Schedule 17 of the Act.

2. RECOMMENDATION

APPROVAL

INFORMATIVES

1. I99 Non Standard Informative

The proposed development includes measures relating to protected species. The proposals must be designed in accordance with any relevant protected species licence and ecological standards for the project.

3. CONSIDERATIONS

3.1 Site and Locality

The Harvil Road ecological habitat creation site is located within the northwest corner of a field west of Harvil Road, immediately south of Dews Lane and approximately 900m to the south of South Harefield. The site is 1.6ha in area and lies approximately 110m to the north-east of the HS2 railway line.

The site is shaped roughly in the form of a 'J', with a wider element at the south of the site extending towards the west, following the boundary of the southern portion of Dews Dell Site of Borough Importance (SBI) (Grade I).

The site is bound to the south by an intact, species rich hedgerow, which also contains a drainage ditch. The site is bound to the west and north (beyond Dews Lane) by an area of broad leaved woodland and to the east by an intact, species poor hedge separating the site from Harvil Road.

A lake is located approximately 350m west of the site which is classified as a Site of Metropolitan Importance for Nature Conservation (SMI).

To the south of the site is further arable field, which is bounded by the Chiltern Main Line, approximately 290m to the south of the site.

The Frays Valley Local Nature Reserve (LNR) is located approximately 580m south-west of the site.

In terms of protected species, bat and badgers are known to use the area and a field south of the site is considered to be suitable otter terrestrial habitat.

The site has been subject to recent planting and earthworks (including the creation of ponds basking banks and hibernacula) to provide ecological mitigation for HS2 and to be available as a receptor site for translocating reptiles if required.

3.2 Proposed Scheme

This application relates to the request for approval of plans and specifications relating to above ground permanent works pursuant to Schedule 17 of the Act. (Paragraphs 2 of Schedule 17: - The erection, construction, alteration or extension of any building (as defined in paragraph 30)

The works submitted for approval comprise a single storey bat house at the southern end of the site, with a footprint of approximately 8.8m x 10.2m and a height of 5.9m to the ridge of the pitched roof. The bat house will be constructed in blockwork cavity walls, clad with waney edge timber boarding incorporating bat access bricks and a red clay tile roof with two solar panels.

The relevant scheduled work as set out under Schedule 1 of the Act to which this Schedule 17 submission relates is the Colne Valley viaduct (forming part of Work No. 2/1). This is subject to a separate current Schedule 17 Plans and Specifications submission (LB Hillingdon ref. 74320/APP/2019/3187).

Other main consents likely to be required for the works include Ecological Licenses and , Schedule

17: Bringing Into Use.

3.3 Relevant Planning History

The High Speed Rail (London-West Midlands) Act 2017 received Royal Assent on 23rd February 2017 and contains 70 sections and 33 schedules. Section 20 provides that planning permission is deemed to be granted under Part 3 of the Town & Country Planning Act 1990 (TCPA 1990) for development authorised by the Act.

The nominated undertaker, HS2 Ltd, is required to attain relevant approvals from Hillingdon Council which is designated as the 'Qualifying Authority' in accordance with the schedule 17 (s17) of the Act.

The purpose of the schedule 17 is to put into place a process for the approval of certain planning matters relating to the design and construction of HS2. This helps to ensure that there is an appropriate level of control over construction works, but without imposition of undue delay or cost to the project. It is noted that the planning grounds for determination are more constrained under the HS2 Act as compared to the TCPA 1990.

Schedule 17 of the Act sets out the grounds on which a relevant planning authority may impose conditions on approvals, or refuse to approve the requests for approval. Henceforth, the Council can only refuse requests for approval or impose conditions in accordance with the relevant grounds as set out per S17, which requires the agreement of the nominated undertaker (HS2 Ltd) in the case of lorry route submissions.

The Planning Conditions set out in Schedule 17 of the Act require the nominated undertaker to submit requests for approval to the relevant planning authority for:

- Building works; (subject to this application)
- Other construction works;
- Matters ancillary to development (referred to as construction arrangements);
- bringing into use;
- site restoration schemes;
- waste and soil disposal and excavation; and
- road transport (lorry route approval).

This submission relates to the Harvil Road ecological mitigation site which has previously been subject to a Schedule 17 Plans and Specifications approval for earthworks, including two mitigation ponds, one hibernaculum and one reptile bank, together with boundary fencing (LB Hillingdon ref. 73195/APP/2017/3486).

4. ADVERTISEMENT AND SITE NOTICE

4.1 Advertisement Expiry Date: Not Applicable

4.2 Site Notice Expiry Date: Not Applicable

5.0 PLANNING POLICES AND STANDARDS

The following Local Plan Policies are considered relevant to the application. In so far as this application is concerned the most pertinent policies applicable to the proposals relate to Green Belt, Biodiversity and Flood Risk Management.

Part 1 Policies:

1. **PT1.EM2 (2012) Green Belt, Metropolitan Open Land and Green Chains**

(2012) Green Belt, Metropolitan Open Land and Green Chains

2. **PT1.EM6 (2012) Flood Risk Management**

(2012) Flood Risk Management

3. **PT1.EM7 (2012) Biodiversity and Geological Conservation**

(2012) Biodiversity and Geological Conservation

4. **PT1.EM8 (2012) Land, Water, Air and Noise**

(2012) Land, Water, Air and Noise

Part 2 Policies:

1. **DMEI 4 Development on the Green Belt or Metropolitan Open Land**

Development on the Green Belt or Metropolitan Open Land

2. **DMEI 7 Biodiversity Protection and Enhancement**

Biodiversity Protection and Enhancement

3. **DMHB 1 Heritage Assets**

Heritage Assets

4. **DMHB 14 Trees and Landscaping**

Trees and Landscaping

5. **DMT 1 Managing Transport Impacts**

Managing Transport Impacts

6. **DMT 2 Highways Impacts**

Highways Impacts

7. **LPP 5.12 (2016) Flood risk management**

(2016) Flood risk management

8. **LPP 7.16 (2016) Green Belt**

(2016) Green Belt

9. **LPP 7.19 (2016) Biodiversity and access to nature**

(2016) Biodiversity and access to nature

10. **LPP 7.21 (2016) Trees and woodlands**

(2016) Trees and woodlands

11. **NPPF National Planning Policy Framework**

National Planning Policy Framework

6.0 COMMENTS ON PUBLIC CONSULTATION

6.1 HISTORIC ENGLAND (GLAAS)

I have now received and considered the written statement which accompanied this application.

The statement acknowledges the archaeological interest in the wider area between Dews Farm and Harvil Road and summarises the extensive archaeological investigations which have taken place so far. Copies of the report referred to in paragraphs 2.1.10 have been supplied to GLAAS as part of our ongoing advisory role and I have also undertaken monitoring visits to view recent investigations for which no report is yet available. The actual location of the proposed bat house has not been subject to archaeological evaluation trenching or excavation partly due to the proximity of a badger sett.

The written statement concludes that the bat house is not located within an area with a high potential for Late Neolithic/Bronze age remains and that the localised extent of the proposed construction impact is unlikely to provide a significant or meaningful contribution to the knowledge creation objectives set out in the GWSI:HERDS. I think these are reasonable conclusions and therefore do not consider that these building works ought to be modified to preserve a site of archaeological interest.

This response relates solely to archaeological issues.

NATURAL ENGLAND

NO OBJECTION

Based on the plans submitted, Natural England considers that the proposed development will not have significant adverse impacts on statutorily protected sites or species.

Natural England has been consulted on the proposals prior to submission and has discussed the nature of the works with HS2. We have provided the summarised comments below directly to HS2 as part of this process and do not have any further comments to make.

- Satisfied with the orientation of the building and its access points
- The building must be a dedicated bat house and no dual function; and therefore, heavy security doors must be fitted to prevent unauthorised access. The roof void should be uncluttered by roof trusses. Long-eared bats tend to roost in clusters along the exposed ridge beam and benefit from the installation of squeeze boxes and a pocketed roosting area.
- Ensure gaps in the lockable metal grille into the roof void has 300mm gaps to allow bats to fly through.
- The number of bat access points should be reduced from 10 to reduce air flow in the roof void.
- Breathable roof membranes should be installed in any part of the building.

Natural England's advice on other natural environment issues is set out below:

This planning proposal is for a development scheme or works scheduled under the provisions of the High Speed Rail (London-West Midlands) Act (2017) which form part of the High Speed Two Scheme within your area. It should therefore be determined using the planning regime established by that legislation. The Act grants the work deemed planning permission, subject to certain matters and details of the deemed consent being reserved for subsequent local planning authority approval under Schedule 17.

We advise that, in determining the consultation, the planning authority should have regard to the permissions already granted under The Act, and to any relevant supporting documents to The Act.

Bats:

Natural England has developed two HS2 specific Bat Mitigation Class Licences. One licence covers certain bat species and roost types in built structures (WML-CL39) and the other covers certain bat species and roost types in trees (woodland, parkland or single tree situations)(WML-CL40). These licences permit suitably experienced employees and staff of contractors to undertake specified activities affecting bats that would otherwise be unlawful. The licence facilitates the enabling and construction works for the high speed rail line between London and Birmingham (Phase 1). In addition, HS2 can also use the existing Bat Mitigation Class Licence (WML-CL21) or apply to Natural England for a site specific licence if the species, scale of impact or roost type are not covered by the Class Licences. The planning authority should advise HS2 Ltd that the proposals must be designed in accordance with the relevant licence and ecological standards for the project.

6.2 SUSTAINABILITY OFFICER

Comments have been incorporated in the report.

7.0 MAIN PLANNING ISSUES - High Speed Rail(London - West Midlands) Act

7.1 BUILDING WORKS

In accordance with Schedule 17 of the Act, the relevant planning authority may only refuse to approve plans or specifications on defined grounds. Paragraph 2 of Schedule 17 outlines the grounds for determination for 'building works', which in this submission are for the erection of a bat house.

The grounds for determination under Paragraph 2 of Schedule 17 are as follows:

- (a) the design or external appearance of the building works ought to be modified
 - (i) to preserve the local environment or local amenity,
 - (ii) to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area, or
 - (iii) to preserve a site of archaeological or historic interest or nature conservation value, and is reasonably capable of being so modified, or
- (b) the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits.

The proposal is required to offset harm to buildings being demolished alongside Dews Lane north of the site in order to construct the HS2 viaduct. These predominantly brick built buildings are of a type and age that would support bat populations. It is understood that bat surveys revealed the presence of bats in some of these properties.

The proposed development is a bespoke brick built structure design purposefully to offset the impacts of the loss of the Dews Lane buildings.

The proposed development needs to be in broad proximity to the Dews Lane buildings to support the same bat populations. It is considered by officers that there would be a degree of flexibility as to the specific location.

DESIGN

It is considered that the visual impact of the 'building works' has been limited by the careful design of materials, locations and screening to maximise the integration of the works into the local environment. The design of the bat house seeks to replicate the features of the buildings to be demolished, to provide the greatest prospect of the proposed building being used as a bat roost. This includes providing clear void uncluttered roof space (no truss rafters) with a range of conditions to meet Natural England requirements.

In terms of appearance, the intention is for the proposed bat house seek to reflect the local vernacular of agricultural outbuildings, using materials typical of the locality (red rustic brick with a red clay tiled roof) to ensure that it would respect the character of its surroundings.

In terms of visual impact, the bat house would be sited such that it would be heavily screened by adjacent woodland and hedge boundaries and would therefore have little or no visual influence outside the immediate field.

With regard to landscaping, the applicant advises that to ensure that appropriate bat flightline and foraging connectivity is provided to the bat house from the existing Dews Farm building and bat foraging areas, new hedgerow and tree planting along Dews Lane and the Newyears Green Bourne is being coordinated with the wider landscaping masterplan for the area. The timing of these landscaping works may be brought forward, or temporary hedgerows provided as an interim solution.

It should be noted that these landscaping works do not require Schedule 17 approval, although they will form part of the mitigation proposals and will be relevant to the future Schedule 17 Bringing Into Use submission for the relevant scheduled works. The Council will be consulted on these landscaping proposals when finalised.

Therefore, it is not considered that the design or external appearance of the works ought to be modified to preserve local environment or amenity.

ECOLOGY

There are existing buildings at Dews Farm which are to be demolished to facilitate the construction of the Colne Valley Viaduct. Ecological surveys have revealed the presence of brown long-eared bat (*Plecotus auritus*) maternity and hibernation roost along with two single pipistrelle bat (*Pipistrellus Pipistellus*) day roosts. In response to Natural England requirements and HS2 Technical Standards, a bat house is required to mitigate for the loss of the existing roosts.

Officers acknowledge that the siting and position of the bat building needs to be as close as possible to the existing roost and within or adjacent to commuting and foraging habitat. The bat building has been positioned away from areas which will be affected by construction works in order to minimise the potential for disturbance. Taking into account these factors has led the applicant to determine that the existing ecological mitigation site at Harvill Road provides the optimal location. Officers do not consider that the development ought to, and could reasonably be carried out elsewhere within the development's permitted limits.

Natural England raises no objections to the proposed building works and considers that the proposed development will not have significant adverse impacts on statutorily protected sites or species.

Officers acknowledge that the bat house will provide a valuable habitat in this area and will assist in the long term conservation of important species. It is considered that the proposals would not have a detrimental impact on a site of ecological value (i.e. a designated site).

It is open to the Council to seek an alternative location for the bathouse should it be deemed that it ought to and could reasonably be located at an alternative location. This is set out in Schedule 17(2)(5)(b). However, as set out above, officers have no objections to the bathouse in the proposed location and it is deemed to be appropriate mitigation for the impacts resulting from the demolition of buildings on Dews lane.

HERITAGE

The site lies on a south facing slope above the Newyears Green Bourne within HS2 Community Forum Area 7, Archaeological Character Areas (ACA) 07-01 (East Side of Colne Valley) and ACA 07-02 (Terrace Gravels within Colne Valley) and the Colne Valley Archaeological Priority Zone as defined by the London Borough of Hillingdon and Historic England's Great London Archaeological Advisory Service (HE GLAAS).

The applicant advises that between January 2018 and May 2020, a number of archaeological investigations have been completed within the site. The written statement supporting this submission concludes that the bat house is not located within an area with a high potential for Late Neolithic/Bronze age remains and that the localised extent of the proposed construction impact is unlikely to provide a significant or meaningful contribution to the knowledge creation objectives.

GLAAS considers that the proposal will not have a significant effect on heritage assets of archaeological interest and is satisfied that the question of amending the design to preserve an archaeological site does not arise.

It is therefore not considered that the works ought to or could reasonably be located elsewhere within the development's permitted limits, or that the works are reasonably capable of being modified to preserve a site of archaeological or historic interest.

HIGHWAY ISSUES

A Public Right of Way (PRoW) (U34) runs along the southern boundary of the field in which the site located. This will not be affected by thhe proposals.

The site is connected to the public highway, with Dews Lane located immediately north and Harvil Road located immediately east of the site. Harvil Road connects with the B467 to the south and subsequently provides access to the A40, the M40 and M25.

For construction of the bat house and future infrequent operational maintenance, the site will be accessed through the existing agricultural access, located along the northern boundary, leading onto Dews Lane.

A plan has been submitted plan indicating the route that maintenance vehicles would take to access the Harvil Road Bat House on an infrequent basis. This is not for approval but is provided to inform officers and Members of the Planning Committee. It is not considered that the works will have any detrimental impact on road safety or the free flow of traffic in the local area.

8.0 BOROUGH SOLICITOR COMMENTS

The High Speed Rail Act 2017 received Royal Assent on 23 February 2017. Section 20 of the Act provides that planning permission is deemed to be granted under Part 3 of the Town and Country Planning Act 1990 for development authorised by the Act, subject to the other provisions of the Act and the conditions set out in Schedule 17. It is a condition of the deemed planning permission that the development must be begun no later than the end of 10 years beginning with the date on which the Act is passed.

This is a straightforward Schedule 17 application which is governed by paragraph 2 of the said Schedule which is headed, 'Condition relating to building works'. There is a requirement that development consisting of building works, which are the responsibility of HS2 Ltd, must be carried out in accordance with plans and specifications for the time being approved by the relevant planning authority.

"Building works" are defined as the erection, construction, alteration or extension of any building, other than a temporary building.

The Council, which is a qualifying authority for the purpose of Schedule 17, is only permitted to refuse to approve the plans or specifications, or to impose conditions on approval of them, on one or more of the prescribed statutory grounds which Members of the Sub-Committee are now familiar with.

The grounds are that the design or external appearance of the building works ought to be modified -

- . to preserve the local environment or local amenity,
 - . to prevent or reduce prejudicial effects on road safety or on the free flow of traffic in the local area, or
 - . to preserve a site of archaeological or historic interest or nature conservation value, and is reasonably capable of being so modified, or
- the development ought to, and could reasonably, be carried out elsewhere within the development's permitted limits.

As is made clear in the body of the report, none of these grounds are engaged in relation to this particular Schedule 17 application so there are no legal reasons to prevent Members from approving it.

9.0 OTHER ISSUES

FLOOD RISK

The site is located within Flood Zone 1 and is not an area identified as at risk of flooding.

FUTURE OWNERSHIP / MAINTENANCE

Whilst this is not a matter which is relevant to the Schedule 17 determination, the the applicant advises that the bat house will be subject to an Environmental Site Management Plan, which will set this out in due course. The bat house will remain a HS2 asset and will be maintained by HS2 after construction until agreements are in place with the landowner or a third party.

10.0 REFERENCE DOCUMENTS

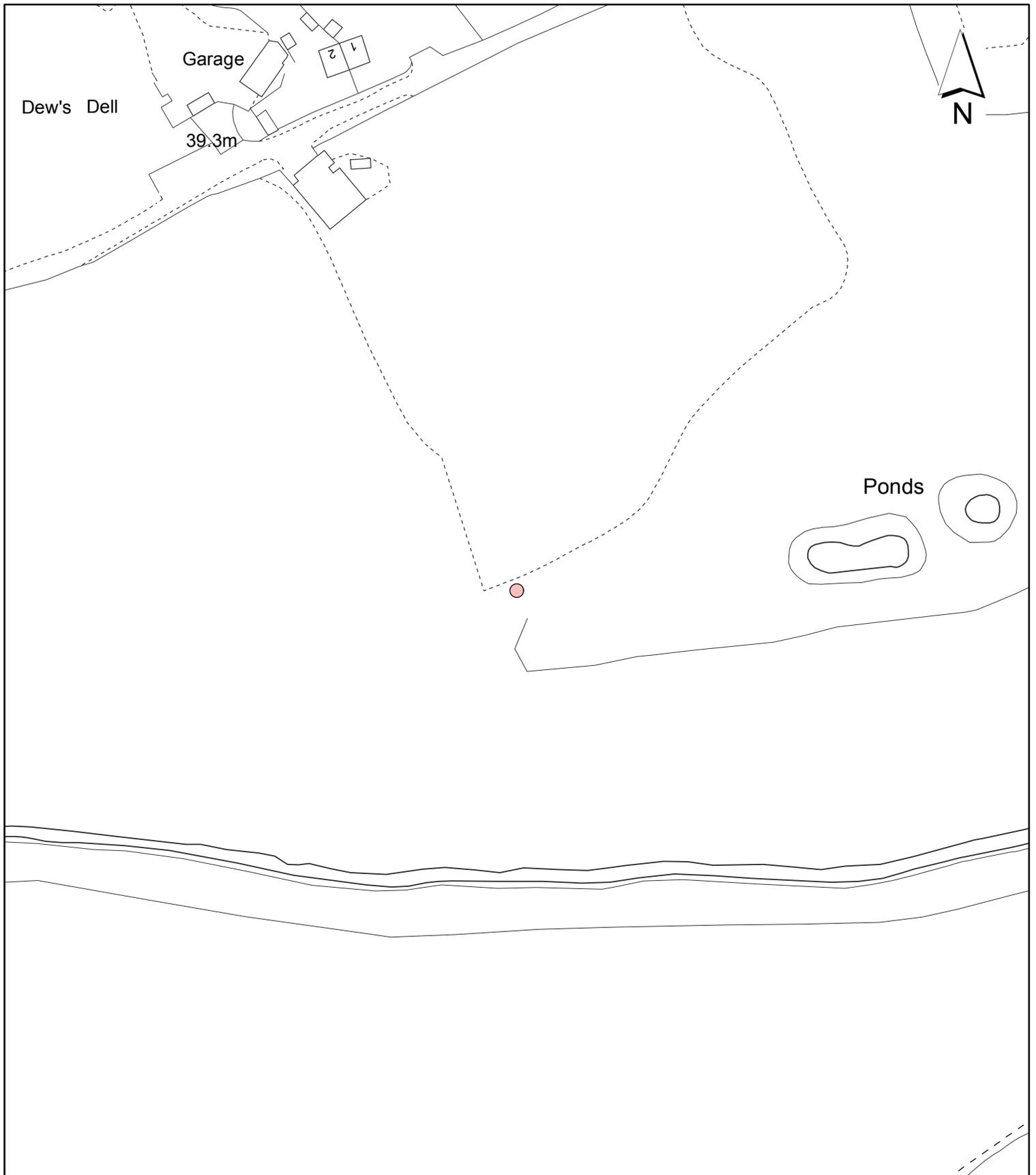
The High Speed Rail (London-West Midlands) Act 2017.

Contact Officer:

Karl Dafe

Telephone No:

01895 250230



Notes:

 Site boundary

For identification purposes only.
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Site Address:

**ECOLOGICAL MITIGATION SITE
 HARVIL ROAD**

Planning Application Ref:

73195/APP/2020/1946

Scale:

1:1,250

Planning Committee:

HS2 Page 17

Date:

October 2020

**LONDON BOROUGH
 OF HILLINGDON
 Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 01895 250111



HILLINGDON
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Agenda Item 7

Item No. Report of the Head of Planning and Enforcement

Address: BETWEEN RIVER COLNE & G.U.CANAL NORTH OF DENHAM LOCK
UXBRIDGE

Development: Request for approval of 'bringing scheduled works and depots into use' (Paragraph 9) under Condition imposed by Schedule 17 to the High Speed Rail (London - West Midlands) Act 2017 relating to the diversion of the National Grid ZC overhead power line in the Colne Valley .(Schedule 1 Work No. 2/4B) -a temporary bridge over the River Colne).

LBH Ref Nos: 42471/APP/2020/2631

Drawing Nos:	Date of Plans:
51350/3913 Sheet 1 of 2 Issue A	24-08-2020
ZC River Colne Crossing Ecology Note_Final	02-10-2020
Flood Risk Assessment	02-10-2020
D-K103975107-H-002 Rev 01	24-08-2020
Sch 17(9) LBH Covering Letter ZC P03 20082020	20-08-2020
River Colne Bridge Block Plan	24-08-2020
Ecological Method Statement Issue 2 Version 4	14-10-2020
River Colne Temporary Bridge location plan	24-08-2020
D-K103975107-H-001 Rev 01	24-08-2020
D-K103975107-H-003 Rev 01	24-08-2020
Sch 17(9) Supporting Statement P03 24072020	24-08-2020
51350/3913 Sheet 2 of 2 Issue A	24-08-2020
WFD Compliance Form	02-10-2020

Drawing Nos:	Date of Amended Plans:
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Date Application Valid: 20th August 2020

1. SUMMARY

This application comprises a bringing into use submission under Schedule 17 of the High Speed Rail (London-West Midlands) Act 2017 (The Act), in relation to National Grid's requirements to modify its existing overhead line network on the 'ZC' route, to allow construction of the High Speed 2 railway line within the Colne Valley, where the railway crosses the existing National Grid ZC overhead line on a proposed viaduct.

The details relate purely to a temporary bridge (known as 'Scheduled Works' No. 2/4B), required over the River Colne to access worksites within Denham Country Park, where two pylons are to be removed and one pylon constructed.

The nominated undertaker (HS2 Ltd) is authorised to construct and maintain the 'Scheduled Works' for Phase One of High Speed 2. However, the Planning Conditions set out in Schedule 17 of the Act require the nominated undertaker to submit requests for approval to the relevant planning authority for bringing into use most 'Scheduled Works' and railway depots. The purpose of 'bringing into use' approvals is to ensure the nominated undertaker for the work takes all reasonably practical measures for the mitigation of the work.

The application is the second HS2 Schedule 17 Request for approval of 'Bringing into Use' submissions that has been deposited with the Council. These Schedule 17 planning submissions can best be likened to the submission of reserved matters, where outline planning consent has already been granted. However, the role of the Planning Authority is heavily restricted as to what can and cannot form the basis of a decision.

This application is slightly different to the previous bringing into use submission. The development relates to a building as defined by Schedule 17(30) which encompasses 'any structures' (with several exceptions).

Buildings are generally captured by Schedule 17(2) and would require consent prior to development taking place and then Schedule 17(8) would require further consent before being brought into use (assuming the building relates to a scheduled work). However, Schedule 17(2) specifically omits temporary buildings from the need for consent. Therefore, in this instance the Council has not had to consent the works prior to them taking place; the normal safeguards allowed for under Schedule 17(2), i.e. to preserve a site of nature conservation interest have not been addressed prior to the temporary works taking place.

Unusually though, the temporary bridge is a scheduled work as listed in Schedule 1 (Work 2/4b). Consequently, consent is required from the Council prior to it being brought into use, but importantly, after the construction.

The Council is therefore being asked to consider the effects of the installation and operation of the bridge and importantly whether mitigation has been appropriately secured in relation to both these factors.

There is no statutory obligation to consult with neighbours. However, the Environment Agency, Natural England and GLAAS have been consulted. No objections have been raised by these statutory bodies to the Schedule 17 submission.

It is considered that there are no reasonably practicable measures which need to be taken for the purpose of mitigating the effect of the work or its operation in terms of its impact on the local environment / local amenity, in accordance with Paragraph 9 sub section (4)(a) of Schedule 17 of the Act.

2. RECOMMENDATION

APPROVAL

INFORMATIVES

1. I99 Non Standard Informative

The proposed development includes measures relating to protected species. The proposals must be designed in accordance with any relevant protected species licence and ecological standards for the project.

3. CONSIDERATIONS

3.1 Site and Locality

The works take place to the west of Harvil Road Only access works are to take place within Buckinghamshire Council, to the south of Buckinghamshire Golf Club within Denham Country Park. The main works and all new pylon are within London Borough of Hillingdon.

The temporary bridge will span the River Colne commencing at a point 200 metres north-east of the junction of footpaths DEN/20/2 and DEN/20/1 with bridleways DEN/48/2 and DEN/48/3 and terminating at a point 134 metres north-east of its commencement.

3.2 Proposed Scheme

The purpose of this submission is to seek the approval of the qualifying authority under Schedule 17 to bring into use HS2 works set out below.

The Scheduled Work subject to this submission relates to a temporary bridge (known as 'Scheduled Works' No. 2/4B), required over the River Colne to access worksites within Denham Country Park, where two pylons are to be removed and one pylon constructed.

Scheduled Work No. 2/4B is described as follows:

" A temporary bridge over the River Colne commencing at a point 200 metres north-east of the junction of footpaths DEN/20/2 and DEN/20/1 with bridleways DEN/48/2 and DEN/48/3 and terminating at a point 134 metres north-east of its commencement"

Schedule 1 to the High Speed Rail (London - West Midlands) Act 2017 sets out the 'Scheduled Works' the nominated undertaker is authorised to construct and maintain for Phase One of High

Speed 2. Paragraph 9 of Schedule 17 to the Act requires the nominated undertaker to submit a bringing into use request for most 'Scheduled Works' and railway depots

The temporary River Colne bridge has a solid bridge base and includes sideboards/barge boards to prevent debris for vehicles entering the watercourse.

Bridge abutments are set back from the river banks in order to provide an offset to River Colne watercourse which will enable access for environmental staff and ecologists access to watercourse at/under the bridge for the implementation of an operational monitoring regime at all times.

The temporary bridge is required for a period of up to 15 months. In due course a Schedule 17(12) 'Site Restoration' submission will be made to London Borough of Hillingdon and Buckinghamshire Council which will include the removal of the bridge (together with supporting bridge abutments) over the River Colne.

3.3 Relevant Planning History

The High Speed Rail (London-West Midlands) Act 2017 received Royal Assent on 23rd February 2017 and contains 70 sections and 33 schedules. Section 20 provides that planning permission is deemed to be granted under Part 3 of the Town & Country Planning Act 1990 (TCPA 1990) for development authorised by the Act.

The nominated undertaker, HS2 Ltd, is required to attain relevant approvals from Hillingdon Council which is designated as the 'Qualifying Authority' in accordance with the schedule 17 (s17) of the Act. The purpose of the schedule 17 is to put into place a process for the approval of certain planning matters relating to the design and construction of HS2. This helps to ensure that there is an appropriate level of control over construction works, but without imposition of undue delay or cost to the project. It is noted that the planning grounds for determination are more constrained under the HS2 Act as compared to the TCPA 1990.

Henceforth, the Council can only refuse requests for approval or impose conditions in accordance with the relevant grounds as set out per S17.

The Planning Conditions set out in Schedule 17 of the Act require the nominated undertaker to submit requests for approval to the relevant planning authority for:

- Building works;
- Other construction works;
- Matters ancillary to development (referred to as construction arrangements);
- bringing into use; (subject to this submission)
- site restoration schemes;
- waste and soil disposal and excavation; and
- road transport (lorry route approval).

Schedule 17 of the Act sets out the grounds on which a relevant planning authority may impose

conditions on approvals or refuse to approve the requests for approval.

The bringing into use is the point at which the Council can determine the efficacy of the mitigation for the effect of the project.

Specific Site History

National Grid is required to modify its existing overhead line network on the 'ZC' route to allow construction of the High Speed 2 railway line within the Colne Valley, where the railway crosses the existing National Grid ZC overhead line on a proposed viaduct. Increased safety clearances are required for the construction and, once built, operation of the High Speed 2 railway on top of this viaduct giving rise to the requirement to move the overhead line.

The National Grid works include the removal of five existing pylons and the construction of six new pylons alongside re-wiring of overhead line on the new arrangement.

A temporary bridge (known as Work No. 2/4B) is required over the River Colne to access worksites within Denham Country Park where two pylons are to be removed and one pylon constructed.

The temporary bridge is authorised by the HS2 Act and is given deemed Planning Permission. National Grid however requires the consent of London Borough of Hillingdon and Buckinghamshire Council to use the bridge and are required to make this 'Bringing into Use' submission to the authorities under Schedule 17(9) of the HS2 Act.

Being temporary, the bridge does not require Schedule 17 plans & specification approval. However a Schedule 17 lorry route submission has been approved under application ref:74320/APP/2020/22.

The authorities are limited in their considerations of the submission to matters specified within the HS2 Act, namely impacts upon the local environment and local amenity, archaeology and historic interest and nature conservation.

4. ADVERTISEMENT AND SITE NOTICE

4.1 Advertisement Expiry Date: Not Applicable

4.2 Site Notice Expiry Date: Not Applicable

5.0 PLANNING POLICES AND STANDARDS

The following Local Plan Policies are considered relevant to the application. In so far as this application is concerned the most pertinent policies applicable to the proposals relate to Green Belt, Biodiversity and Flood Risk Management.

Part 1 Policies:

1. **PT1.EM2 (2012) Green Belt, Metropolitan Open Land and Green Chains**

(2012) Green Belt, Metropolitan Open Land and Green Chains

2. **PT1.EM6 (2012) Flood Risk Management**

(2012) Flood Risk Management

3. **PT1.EM7 (2012) Biodiversity and Geological Conservation**

(2012) Biodiversity and Geological Conservation

4. **PT1.EM8 (2012) Land, Water, Air and Noise**

(2012) Land, Water, Air and Noise

Part 2 Policies:

1. **DMEI 4 Development on the Green Belt or Metropolitan Open Land**

Development on the Green Belt or Metropolitan Open Land

2. **DMEI 7 Biodiversity Protection and Enhancement**

Biodiversity Protection and Enhancement

3. **DMHB 1 Heritage Assets**

Heritage Assets

4. **DMHB 14 Trees and Landscaping**

Trees and Landscaping

5. **DMT 1 Managing Transport Impacts**

Managing Transport Impacts

6. **DMT 2 Highways Impacts**

Highways Impacts

7. **LPP 5.12 (2016) Flood risk management**

(2016) Flood risk management

8. **LPP 7.16 (2016) Green Belt**

(2016) Green Belt

9. **LPP 7.19 (2016) Biodiversity and access to nature**

(2016) Biodiversity and access to nature

10. **LPP 7.21 (2016) Trees and woodlands**

(2016) Trees and woodlands

11. **NPPF National Planning Policy Framework**

National Planning Policy Framework

6.0 COMMENTS ON PUBLIC CONSULTATION

6.1 NATURAL ENGLAND

This planning proposal is for a development scheme or works scheduled under the provisions of the High Speed Rail (London-West Midlands) Act (2017) which form part of the High Speed Two scheme within your area. It should therefore be determined using the planning regime established by that legislation. The Act grants the work deemed planning permission, subject to certain matters and details of the deemed consent being reserved for subsequent local planning authority approval under Schedule 17.

We advise that, in determining the consultation, the planning authority should have regard to the permissions already granted under The Act, and to any relevant supporting documents to The Act.

Natural England has been consulted on the proposals prior to submission and has discussed the nature of the works with HS2. We have provided the summarised comments below directly to HS2 as part of this process and do not have any further comments to make.

The temporary bridge is upstream of both the Denham Lock Wood SSSI and Fray's Farm Meadow SSSI which are notified for a variety of wetland habitats associated with the floodplain of the Colne Valley. Without protection these habitats could be susceptible to pollution as a result of the installation and operation of the temporary bridge. The supporting statement (Document Reference: UC46-NGT-UT-APP-C001-000008) at paragraph 4.1.3 details measures to avoid and control pollution. Subject to the effective implementation of these measures there should be no detrimental effects on the SSSIs.

Great Crested Newts:

Natural England has issued a route-wide organisational licence for Great Crested Newt (*Triturus cristatus*), a European Protected Species (EPS) to HS2 Ltd. The licence permits suitably experienced employees and staff of contractors to undertake specified activities affecting great crested newts that would otherwise be unlawful. The licence facilitates the enabling and construction works for the high speed rail line between London and Birmingham (Phase 1).

The planning authority should advise HS2 Ltd that the proposals must be designed in accordance

with this licence and ecological standards for the project. Natural England are carrying out licence compliance monitoring to ensure HS2 are meeting the provisions of the licence overall.

Badgers:

Natural England has issued a route-wide organisational licence for Badgers to HS2 Ltd. The licence permits suitably experienced employees and staff of contractors to undertake specified activities affecting badgers that would otherwise be unlawful. The licence facilitates the enabling and construction works for the high speed rail line between London and Birmingham (Phase 1).

The planning authority should advise HS2 Ltd that the proposals must be designed in accordance with this licence and ecological standards for the project. Natural England are carrying out licence compliance monitoring to ensure HS2 are meeting the provisions of the licence overall.

Bats:

Natural England has developed two HS2 specific Bat Mitigation Class Licences. One licence covers certain bat species and roost types in built structures (WML-CL39) and the other covers certain bat species and roost types in trees (woodland, parkland or single tree situations)(WML-CL40). These licences permit suitably experienced employees and staff of contractors to undertake specified activities affecting bats that would otherwise be unlawful. The licence facilitates the enabling and construction works for the high speed rail line between London and Birmingham (Phase 1). In addition, HS2 can also use the existing Bat Mitigation Class Licence (WML-CL21) or apply to Natural England for a site specific licence if the species, scale of impact or roost type are not covered by the Class Licences. The planning authority should advise HS2 Ltd that the proposals must be designed in accordance with the relevant licence and ecological standards for the project.

Proposals affecting multiple protected species:

The proposed development includes measures relating to a number of different protected species. The planning authority should advise HS2 Ltd that the proposals must be designed in accordance with any relevant protected species licence and ecological standards for the project.

Proposals within or affecting the Chilterns AONB:

The proposed development scheme or works is for a site within or close to the Chilterns AONB, which is a nationally designated landscape. Natural England advises that the planning authority uses local landscape expertise and information to determine the proposal. We particularly advise that the proposals should be in line with the aims and objectives of the AONB's statutory management plan.

1, having regard to the provisions of the HS2 Act and Environmental Minimum Requirements. We also advise that the proposal is assessed in terms of whether it meets the complementary Chilterns AONB HS2 Detailed Design Principles

2, having regard to the purposes and status of those Principles.

All public bodies have a duty under Section 85 of the Countryside and Rights of Way Act(2000) to have regard to the statutory purpose of AONBs, which is to conserve and enhance the area's natural beauty. You should assess the application carefully in accordance with the provisions of Schedule 17 of the HS2 Act as to whether the proposed development would have a significant impact on or harm that statutory purpose and, if so, how any proposed mitigation will minimise those effects.

ENVIROMENT AGENCY (EA)

As included in the supporting information, the EA has already issued consent for this activity and thus has no comment.

HISTORIC ENGLAND (GLAAS)

The applicant's written statement does not actually address archaeological impact but it is evident from the construction details that only minor ground works are involved in the construction of the bridge. As there are no known archaeological remains in this location I therefore do not consider that these building works ought to be modified to preserve a site of archaeological interest.

6.2 FLOOD AND WATER MANAGEMENT

This appears to only relate to the temporary bridge over the River Colne and as the lead authority on fluvial flood risk, the Council would expect the Environment Agency to comment on this issue. A separate Schedule 33 application has been made, however there is no detail on the assessment of, or management of flood risk in relation to this application and no information or comment included from the EA approving this proposal.

It is clear that the abutments of the bridge and ramps to it will reduce the flood storage capacity over the life of the temporary bridge. The application does not provide any further detail in terms of access road and drainage arrangements and or where there may be ordinary watercourse crossings that an access road will need to put in place. On the East side of the Colne within Hillingdon there are ordinary watercourses within this area. However an assessment of the need for these consents is not mentioned in the application. It is presumed that this information will be provided under separate consenting requirements should crossings be needed. Reinstatement application is proposed to be made at a later date.

7.0 MAIN PLANNING ISSUES - High Speed Rail(London - West Midlands) Act

7.1 7.1 Bringing Scheduled Works and Depots into Use

The background and general approach to the 'bringing into use' submissions has been set out in the HS2 Planning Forum Note 7 (Bringing into Use Approvals). This indicates that the design of HS2 will seek to blend the operational infrastructure into the landscape and townscape and has generally reduced the effect on the environment. To a large extent, therefore, the mitigation will be an integral part of the design, rather than a separate element.

Paragraph 9, sub section (4) of Schedule 17 (Conditions on bringing scheduled works and depots into use) states that the relevant planning authority must grant approval for the purposes of this paragraph if -

- (a) it considers that there are no reasonably practicable measures which need to be taken for the purpose of mitigating the effect of the work or its operation on the local environment or local amenity, or
- (b) it has approved, at the request of the nominated undertaker, a scheme consisting of

provision with respect to the taking of measures for that purpose.

The relevant planning authority may not refuse to approve, or impose conditions on the approval of a scheme submitted for the purposes of sub-paragraph (4)(b) unless it is satisfied that it is expedient to do so on the ground that the scheme ought to be modified:

- (a) to preserve the local environment or local amenity,
- (b) to preserve a site of archaeological or historic interest, or
- (c) in the interests of nature conservation,

and that the scheme is reasonably capable of being so modified.

This is a request to bring into use a temporary work, so the issue the Council needs to consider is what are the reasonably practicable measures which need to be taken for the purpose of mitigating the effect of the work or its operation on the local environment or local amenity for this temporary work. Officers consider that the relevant issues are the impact on the watercourse and the loss of ecology.

The applicant in its initial submission stated that given that the bridge is a temporary structure and will be removed upon the completion of the National Grid ZC overhead line diversion works, there are very limited impacts associated with the operation of the bridge. The works are required to be undertaken in compliance with the controls set out in the Environmental Minimum Requirements, which includes the High Speed 2 Phase 1 Code of Construction Practice.

HS2 Ltd submits that sufficient controls already exist in the Environmental Minimum Requirements to control the very limited impacts associated with the operation of the bridge such that London Borough of Hillingdon and Buckinghamshire Council can approve the use of the River Colne bridge, Work No. 2/4B, without further mitigation being required.

However, officers raised concerns regarding the applicant's suggestion that the Council only need to consider the operation of the works when it is brought into use, not the mitigation for the effect of the work (i.e. the tree clearance). Officers were of the opinion that this latter position would remove the Council from all consenting of the ecological mitigation for the railway and how the land is to be put back in the Borough. It would reduce the consideration of the railway to essentially the noise from the operation and this would be inconsistent with the Act and the position previously put forward by HS2 Ltd.

In response to these concerns, the applicant has submitted additional information. These documents comprise:

- > Ecological Method Statement (River Colne Ecology Note Issue 2) produced by National Grid which documents the environmental measures associated with the wider ZC line diversion works to ensure potential effects on ecology receptors are avoided and/or minimised.
- > Flood Risk Assessment.
- > Water Framework Directive Compliance Form.
- > Environment Agency Consent

FLOOD AND WATER MANAGEMENT

A Flood Risk Assessment has been submitted which concludes that appropriate mitigation

measures have been proposed to counteract any outlined flood risk, primarily as a series of conveyance pipes proposed to be connected through the temporary access track embankments, to provide suitable management of surface water. Consent for the installation and temporary use of the bridge has already been granted by the Environment Agency.

Furthermore, it must be noted that any risk of flooding resulting from the installation of the bridge is reduced by virtue of the limited period of time it will be in situ. In addition, there are limited sensitive receptors in the near vicinity.

The combination of the limited risk associated with the temporary nature of the works with the limited presence of sensitive receptors satisfies officers that the mitigation secured by the Environment Agency would likely be sufficient.

Key mitigations of potential impact on the watercourse required by the EA consent, in addition to compliance with the above documents, are that the bridge shall, unless otherwise agreed in writing by the Environment Agency, be a minimum 5 metres set back from top of bank of the main river; and that the soffit of the bridge shall be a minimum 35.63mAOD.

ECOLOGY

In terms of the location and routing of the temporary bridge, this forms part of Schedule 1 Work No. 2/4B set out in Schedule 1 to the High Speed Rail (London - West Midlands) Act 2017.

The applicant submits that consideration was given in the design to minimising the impact on ecology. For example the alignment was amended to avoid a veteran common alder on the river bank and an offset based on Forestry Commission. Natural England Standing Advice has been identified to ensure that this veteran tree is protected during the installation and operation and removal of the bridge.

The bridge design was also revised to remove 'in channel' supports in part, on ecology grounds. The ecological assessment submitted with the Schedule 33 EA submission encompassed an approximate 500m stretch of the River Colne corridor (extending 250m upstream and downstream of the bridge) and adjoining bankside habitats. It identifies control measures to minimise the impacts on the designated sites and aquatic species and habitat.

It is acknowledged that localised vegetation clearance is necessary in order to construct the temporary bridge. In relation to replacement planting, the applicant submits that it would not be practicable, reasonable, or accord with best practice to seek to replant around the bridge while it is still in use. Firstly any such replacement planting would have negligible effect in mitigating the bridge itself during the short period of its operation and secondly the process of decommissioning the bridge would be likely to damage any mitigation planting that had been undertaken in its vicinity.

The applicant has provided sufficient ecological information to allow officers to engage on the effect and operation of the scheduled work. An ecological assessment has been provided that sets out the impacts of the installation. The continued presence of the bridge over the temporary 18 month period would have limited operational impacts on the conveyance of water and the impacts on aquatic wildlife.

The principle cause for concern therefore is the loss of vegetation as a consequence of the installation of the bridge. Normally, these impacts would be addressed prior to the development taking place and mitigation secured in advance. In this instance, the consideration is whether the mitigation is satisfactory to offset the harm being caused.

The bridge results in a localised level of ecological harm albeit relatively small in nature and scale compared and of negligible impact in the Colne Valley. Nonetheless, there would be an impact and the purpose of this submission is to determine whether suitable mitigation is and will be put in place to offset the small scale harm caused. To that end there are two sets of mitigation requirements for the installation:

Firstly, the works were undertaken in line with a method statement to ensure minimal harm from the bridge when it was constructed. The scope of impacts are likely to be minimal, and officers are satisfied that the location of the bridge was selected to avoid more sensitive sites. Consequently, the conclusions and recommendations in the assessment are supported and the method statement should provide part of the approved documents.

Secondly, wider restoration of the Colne Valley will be subject to further schedule 17 bringing into use submissions and consequently will be an appropriate avenue for securing additional planting to make up for the relatively small scale harm of this temporary bridge. It would not be prudent to undertake a small section of mitigation planting (minimal square metres) to offset the bridge impacts now when there is a wider plan to secure mitigation on a much grander scale associated with HS2 activity as a whole.

It is considered that the practical and reasonable solution would be to provide a suitable restoration scheme after the bridge has been decommissioned. The HS2 Act envisages and provides for this kind of situation in the site restoration provisions in paragraphs 12 of Schedule 17 and paragraph 5 of Schedule 16, in which a site restoration scheme is required to be agreed with both the landowner and local planning authority prior to giving up possession of the land in question.

It is noted that the Written Statement specifies that the loss of trees and vegetation associated with the creation of a the bridge and wider construction access will be subject to a future site restoration submission to LBH and Buckinghamshire Council under Schedule 16(5).

In conclusion, sufficient information has been presented by the applicant and officers are satisfied that the ecological impacts will be minimised and subject to the removal of the bridge being carried out in accordance with the approved method statement, then there should be limited impacts in the future.

The wider restoration plans for the Colne Valley due to HS2 works will incorporate further mitigation to offset the impacts of this bridge with the intention of no net ecological loss.

Consequently, officers are satisfied that the mitigation proposed satisfactorily offsets the harm of the effects and operation of the scheduled work.

CONCLUSION

In addition to the Environment Agency, both Natural England and Historic England have raised no

objections to this 'Bringing into Use' submission in terms of flood risk, ecology and archaeology.

In conclusion, it is the opinion of officers that there are no reasonably practicable measures which need to be taken for the purpose of mitigating the effect of the work or its operation in terms of its impact on the local environment / local amenity, in accordance with Paragraph 9 sub section (4)(a) of Schedule 17 of the Act.

It is therefore recommended that the 'bringing into use' submission be approved.

8.0 BOROUGH SOLICITOR COMMENTS

Paragraph 9 of Schedule 17 of the High Speed Rail Act 2017 provides that if the relevant planning authority is a qualifying authority, no work to which this paragraph applies may be brought into use without the approval of that authority.

In this instance, the work which this paragraph relates to can be described as scheduled works which are prescribed by Schedule 1 of the HS2 Act. Paragraph 9 applies to all scheduled works, with the exception of work which is underground.

The relevant scheduled work in this instance is Scheduled Work No. 2/4B which relates to a temporary bridge over the River Colne.

The purpose of bringing into use approvals is to ensure that HS2 Ltd takes all reasonably practicable measures for mitigation of the work in question.

Under paragraph 9, the Council must grant approval if:

- . no reasonably practicable measures for mitigating the effect on the environment or local amenity need to be taken, or
- . the Council has approved a mitigation scheme submitted by HS2 Ltd.

The Council may not refuse to approve, or impose conditions on the approval of a mitigation scheme unless it is satisfied that it is expedient to do so on the ground that the scheme ought to be modified - . to preserve the local environment or local amenity,

- . to preserve a site of archaeological or historical interest, or
- . in the interests of nature conservation,

and that the scheme is reasonably capable of being so modified.

The report before Members sets out the main planning issues in relation to this application from HS2 Ltd in Paragraph 7, which concludes that there are no reasonably practicable measures which need to be taken for the purpose of mitigating the effect of the work or its operation in terms of the impact on the local environment and amenity. In the circumstances, this falls squarely within the first limb of paragraph 9, as set out above, and therefore there is no legal reason why Members cannot approve the application.

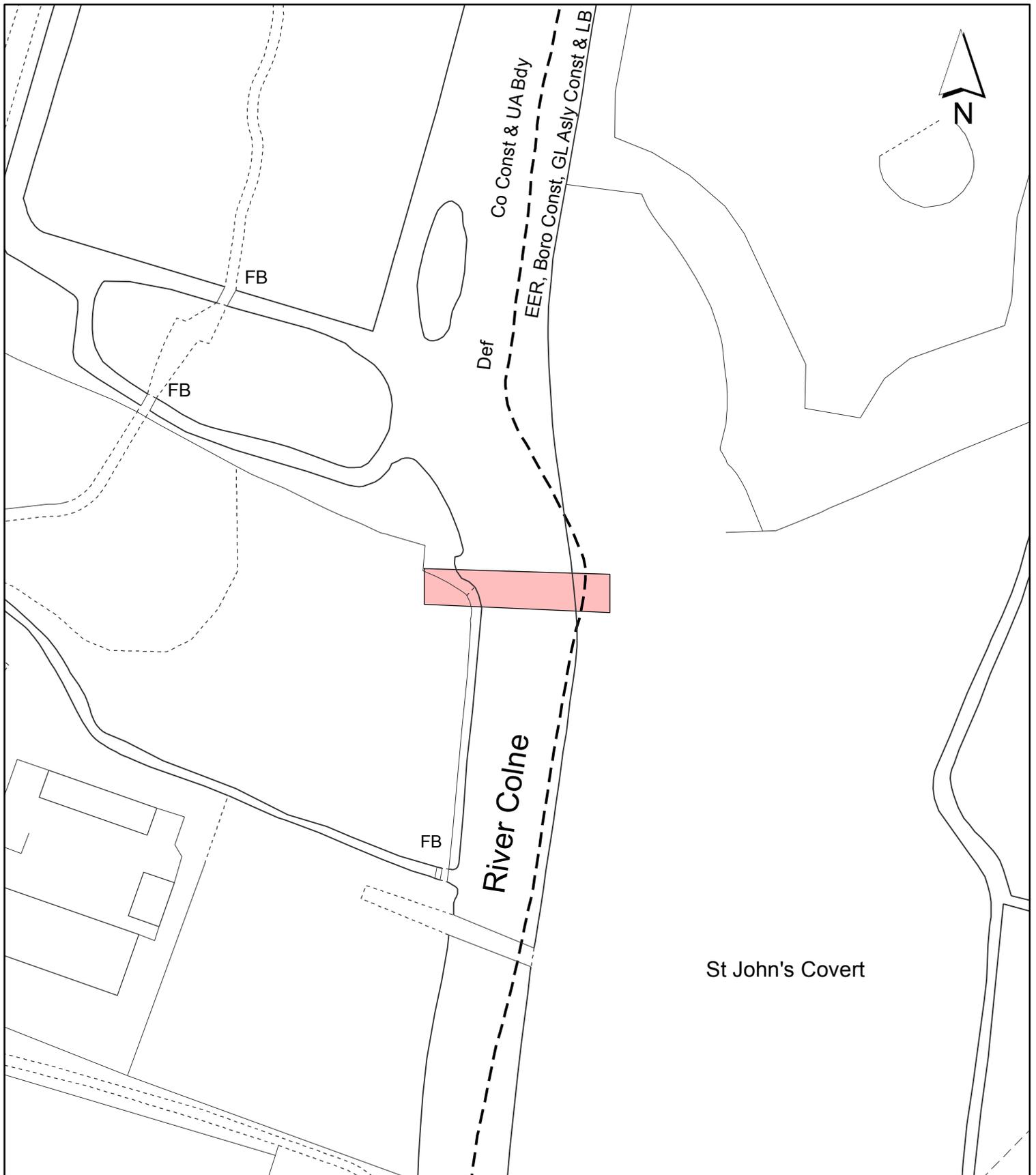
9.0 OTHER ISSUES

None.

10.0 REFERENCE DOCUMENTS

The High Speed Rail (London-West Midlands) Act 2017.
HS2 Planning Forum Note 7 (Bringing into Use Approvals)
Schedule 17 Statutory Guidance Feb. 2017

Contact Officer: Karl Dafe **Telephone No:** 01895 250230



Notes:

 Site boundary

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Site Address:

**BETWEEN RIVER COLNE
 & G.U.CANAL NORTH OF
 DENHAM LOCK**

**LONDON BOROUGH
 OF HILLINGDON**
 Residents Services
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

42471/APP/2020/2631

Scale:

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Planning Committee:

HS2 Page 33

Date:

October 2020



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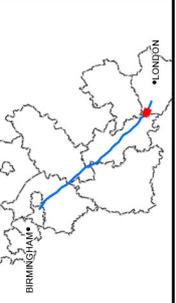
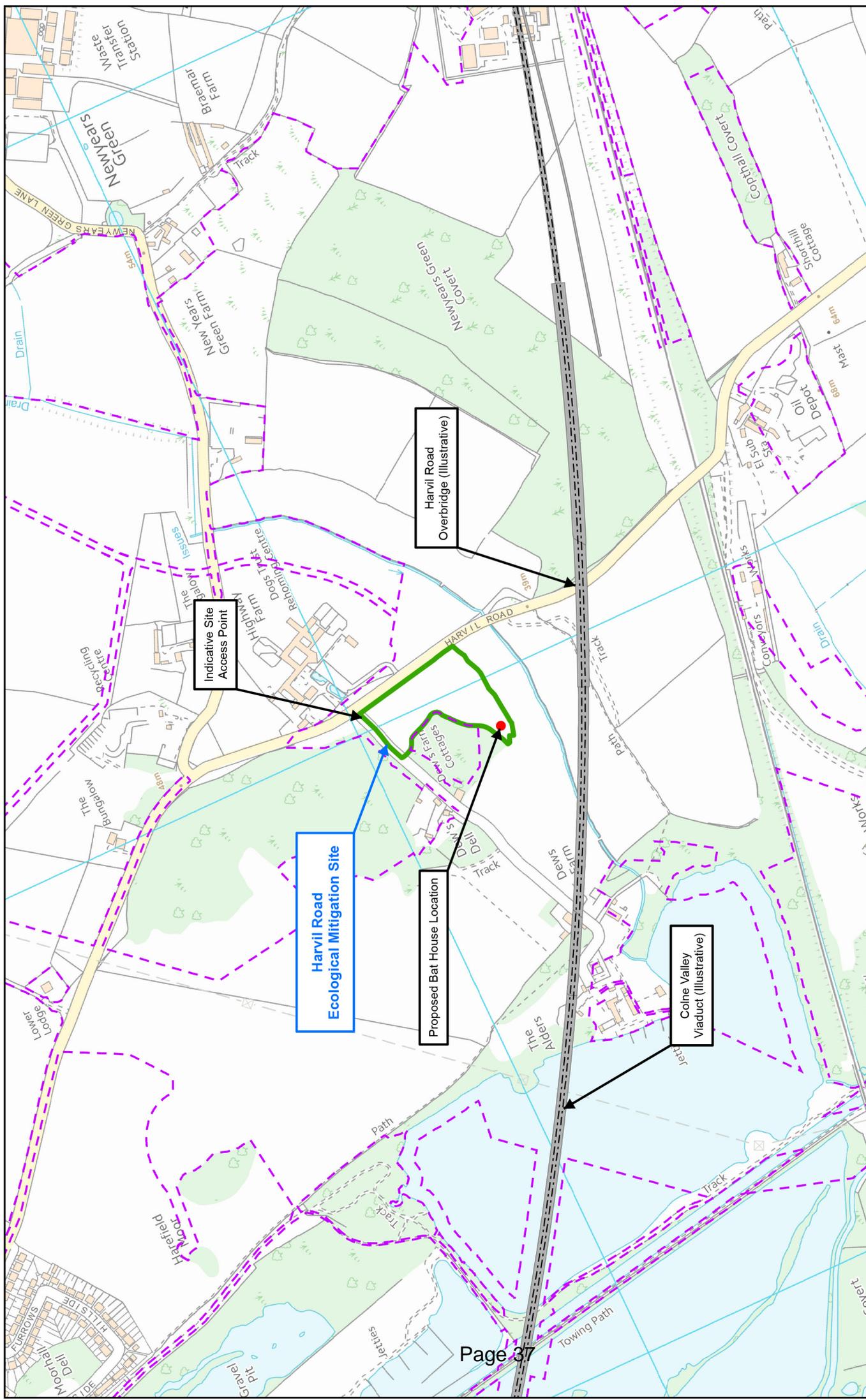
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Plans for HS2 Planning Sub Committee

Thursday 29th October
2020



HILLINGDON
LONDON



Legend

- - - HS2 alignment
- █ Ecological mitigation site boundary
- Proposed Bat House Location
- Outer limit of HS2 Act

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Site Plan
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Legend/Notes:

- Site Boundary

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<p>PROJ: H2S EIA REMOVED</p>	<p>REV: 01</p>	<p>DATE: 15/06/20</p>	<p>BY: [Signature]</p>
<p>PROJ: FOR PLANNING</p>	<p>REV: 02</p>	<p>DATE: 15/06/20</p>	<p>BY: [Signature]</p>
<p>PROJ: [Blank]</p>	<p>REV: [Blank]</p>	<p>DATE: [Blank]</p>	<p>BY: [Blank]</p>

<p>Project/Contract: ENABLING WORKS CENTRAL - EWP1-2103 Description/Title: ENVIRONMENTAL</p>	<p>Zone: COUNE VALLEY VIADUCT & SURFACE WORKS Design Stage: OUTLINE DESIGN Drawn: SL Date: 22/06/20 Checked: MMc Scale: 1:500 Approved: BW Size: A1</p>
<p>Stability Code: S4 Stability: FIT FOR STAGE APPROVAL</p>	<p>Drawing No: 1EW03-FUSEV-DGA-CSDT_CL01-014301 Rev: P02</p>

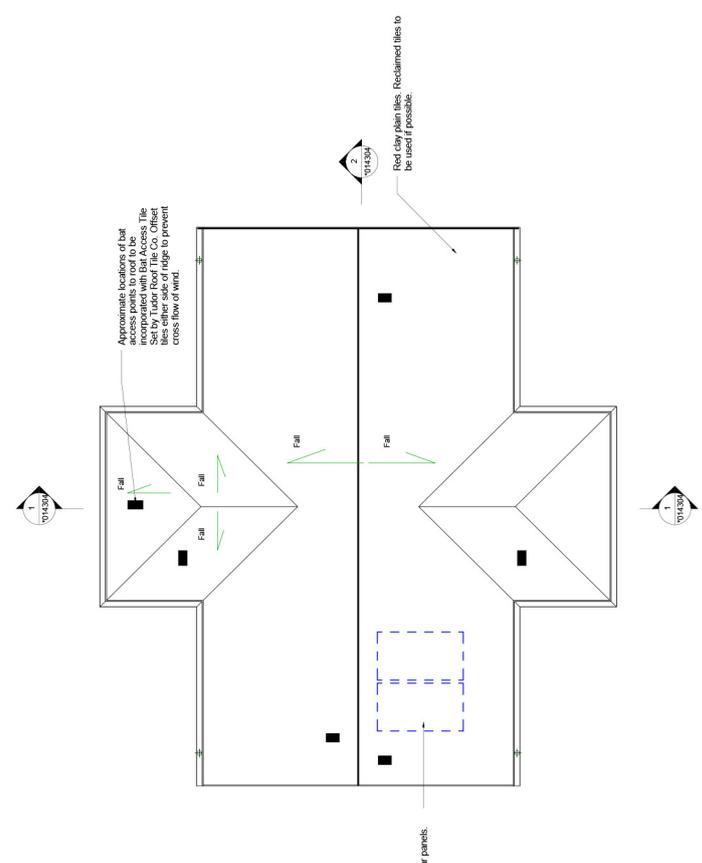


Registered in England
Registration No. 01711696
Registered office:
25 Snow Hill
Birmingham, B4 6GA
Creation/Creation

FUSION

Stability Code: S4
Stability: FIT FOR STAGE APPROVAL

Drawing No: 1EW03-FUSEV-DGA-CSDT_CL01-014301
Rev: P02



1 Ground Floor Plan
1:50

GENERAL NOTES:

- Do not scale from this drawing, use only printed dimensions.
- Heritage brick access bricks to be installed in external blockwork face to allow access into Cool Room. Gap in external blockwork to be filled with concrete. Ply board to be left in situ.
- The contractor shall be responsible for the correct setting out of the works on site, all dimensions to be checked on site prior to fabrication of materials.
- Works shall be carried out in accordance with the latest Building Regulations & all relevant British Standards & Codes of Practice.



2 Roof Plan
1:50

NOTES:

As an alternative to the use of blockwork bat access bricks a rectangular access hole of dimensions 110mm (width) x 25mm (height) may be cut into standard blockwork prior to installation.

<p>Registered in England Registration No. 0971698 2 Silver Hill Brimingham, B4 6CA Customer/Owner: FUSION</p>		<p>Project/Contract: ENABLING WORKS CENTRAL - EMPH-2103 Description/Function: ENVIRONMENTAL</p>		
Zone: COLINE VALLEY VIADUCT & SURFACE WORKS	Design Stage: OUTLINE DESIGN	Drawn: SL	Checked: MJC	Approved: BW
Drawn Title: DENNIS FARM HARVEIL ROAD BAT HOUSE SCHEDULE 17 FLOORS AND SPECIFICATIONS GROUND FLOOR AND ROOF PLANS FOR APPROVAL	Date: 22/06/20	Scale: 1:50	Size: A1	Rev: P102
Stability Code: S4	Subsidiary Code: S4	Fit for Stage: APPROVAL	Drawn No: 1E/WB3-FUS-EV-DCA-CS01_C101-01-H302	Rev: P102



Bat roof access tile



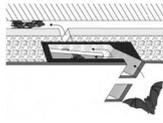
Brickwork to match that of local vernacular



Handcrafted clay plain tiles



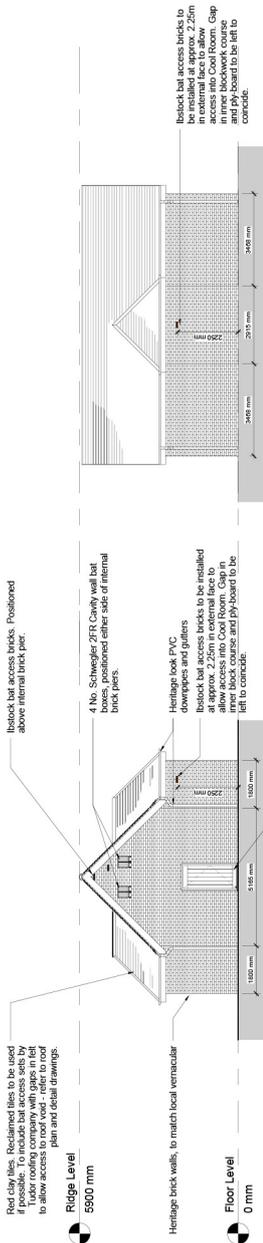
Heritage look PVC rainwater goods



Schwelger ZFR cavity wall bat box



Batbrick bat access brick



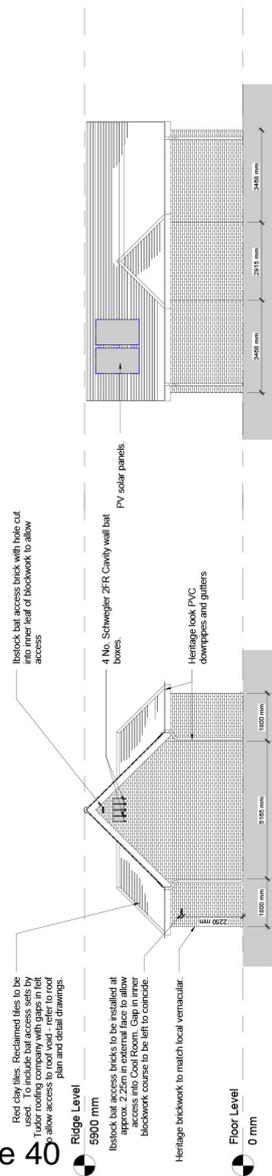
1 North Elevation
1 : 100

Batbrick bat access bricks to be installed at approx. 2.20m in external face to allow access into Cool Room. Gap in inner block course and ply-board to be left to coincide.

Heritage look PVC downpipes and gutters. Batbrick bat access bricks to be installed to match local vernacular. 42mm in row height maximum. Threshold to be no higher than 150mm above ground level.

2 East Elevation
1 : 100

5 Proposed Materials & Bat accessories
N.T.S.



3 South Elevation
1 : 100

Batbrick bat access bricks with hole cut in external face of batbrick to allow access.

Heritage look PVC downpipes and gutters.

4 West Elevation
1 : 100

Ridge tiles: Reclaimed tiles to be used for ridge tiles. Refer to structural drawing for details. Refer to roof plan and detail drawings to allow access to roof void - refer to roof plan and detail drawings.

Batbrick bat access bricks to be installed at approx. 2.20m in external face to allow access into Cool Room. Gap in inner block course to be left to coincide.

Heritage look PVC downpipes and gutters.

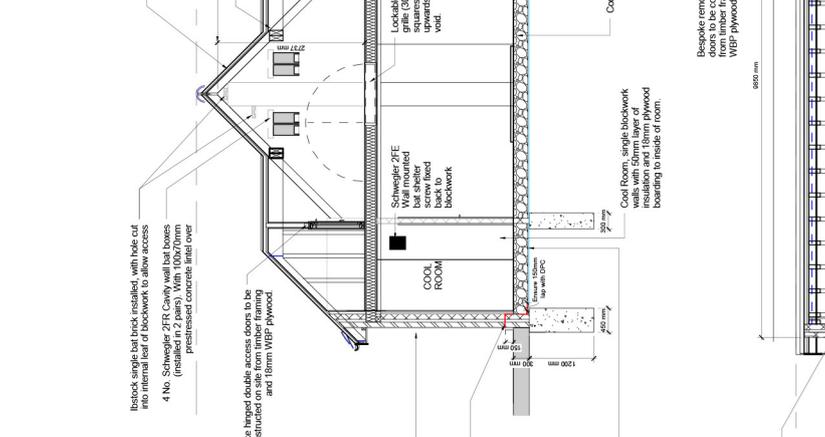
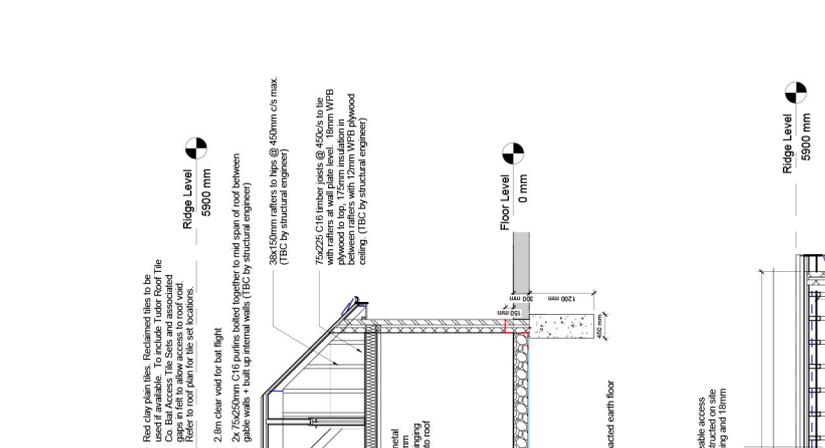


6 3D Image of Proposed Bat House
N.T.S.



<p>Registered in England Registration No. 0971686 2 Storey High Brimingham, B4 6CA</p>		<p>Project/Consult: ENABLING WORKS CENTRAL - EMPH-2103</p> <p>Client/Function: ENVIRONMENTAL</p> <p>Drawn: SL</p> <p>Checked: MJC</p> <p>Date: 22/06/20</p> <p>Scale: 1:100</p> <p>Sheet: A1</p> <p>Revision: 1</p> <p>Rev: IEWB3-FUS-EV-DCA-CS01_C10-01-H303</p>	
<p>Zone: COULNE VALLEY VIADUCT & SURFACE WORKS</p> <p>Design Stage: OUTLINE DESIGN</p> <p>Drawings Title: DENNIS FARM HARVEIL ROAD BAT HOUSE SCHEDULE 17 PLANS AND SPECIFICATIONS ELEVATIONS FOR APPROVAL</p> <p>Stability Code: S4</p> <p>Stability: FIT FOR STAGE APPROVAL</p>		<p>Project/Consult: ENABLING WORKS CENTRAL - EMPH-2103</p> <p>Client/Function: ENVIRONMENTAL</p> <p>Drawn: SL</p> <p>Checked: MJC</p> <p>Date: 22/06/20</p> <p>Scale: 1:100</p> <p>Sheet: A1</p> <p>Revision: 1</p> <p>Rev: IEWB3-FUS-EV-DCA-CS01_C10-01-H303</p>	
<p>PIZ</p> <p>HS2 (EX-REDACTED)</p> <p>PI1</p> <p>PI2</p> <p>PI3</p> <p>PI4</p> <p>PI5</p> <p>PI6</p> <p>PI7</p> <p>PI8</p> <p>PI9</p> <p>PI10</p> <p>PI11</p> <p>PI12</p> <p>PI13</p> <p>PI14</p> <p>PI15</p> <p>PI16</p> <p>PI17</p> <p>PI18</p> <p>PI19</p> <p>PI20</p>	<p>Drawn</p> <p>Checked</p> <p>Approved</p> <p>FUS AEC</p>	<p>Drawn</p> <p>Checked</p> <p>Approved</p> <p>FUS AEC</p>	<p>Drawn</p> <p>Checked</p> <p>Approved</p> <p>FUS AEC</p>
<p>Legend/Notes:</p> <p>All reclaimed materials to be approved by designer prior to use.</p> <p>Refer to Structural Notes on drawings.</p> <p>This drawing is to be read in conjunction with all relevant Architectural, Structural and Civil drawings and their corresponding specifications and schedules of materials.</p> <p>This drawing is to be read in conjunction with the project health & safety file for any identified potential risks.</p>			
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- NOTES**
1. All dimensions in mm unless noted otherwise.
 2. Traditional cut rafter roof comprising of 38 x 150mm timber rafters at 450mm centres.
 3. All timber wall plates to be 47 x 100.
 4. All timber floor joists to be 75 x 220 at 450mm centres. All floor joist to be blocked at third points with minimum 25mm gaps.
 5. Timber joists to be 75 x 250 timbers bolted together. Purlins to be bolted using M12 grade 8.8 bolts with a minimum 300mm overlap.
 6. Timber purlins to be 75 x 250 timbers bolted together. Purlins to be bolted using M12 grade 8.8 bolts with a minimum 300mm overlap.
 7. All components to be treated with a preservative treatment to BS 8445:2015. All components to be treated with a preservative treatment to BS 8445:2015. All components to be treated with a preservative treatment to BS 8445:2015. All components to be treated with a preservative treatment to BS 8445:2015.
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 9. Timber rafters to be bolted using M12 grade 8.8 bolts with a minimum 300mm overlap.
 10. All components to be treated with a preservative treatment to BS 8445:2015. All components to be treated with a preservative treatment to BS 8445:2015. All components to be treated with a preservative treatment to BS 8445:2015. All components to be treated with a preservative treatment to BS 8445:2015.
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1 1 : 50

2 1 : 50

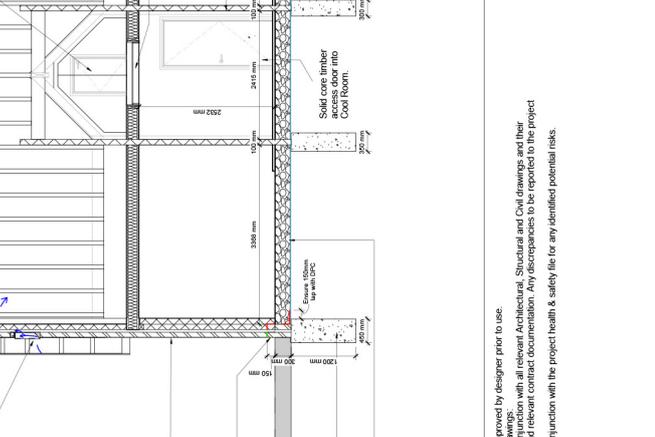
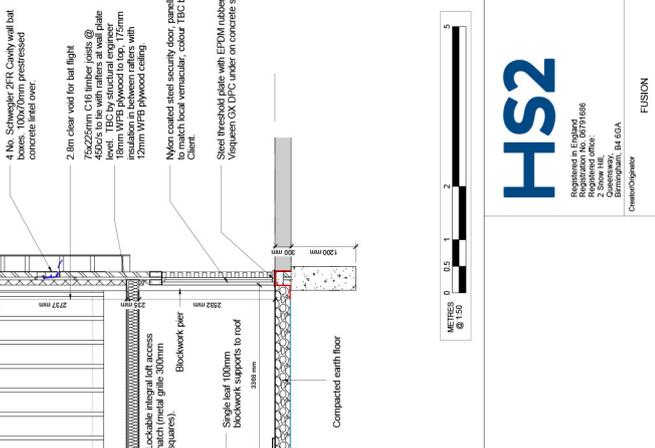
Notes for Cross Section:

- 1. Brickwork exterior wall, heritage bricks to match local vernacular. Reclaimed bricks to be used where possible, subject to approval by the Engineer. With a minimum 100mm gap between bricks. Exterior walls to Cool Room to have 50mm insulation and 18mm WBP plywood boarding.
- 2. Venequeer GX DPC at 150mm above GL, ensure continuous across cavity, foundation level.
- 3. Lay gas protection membrane conforming with BS 8445:2015, BS 8102:2009, and CIRIA C748. Membrane is to be level to top of foundations and led into foundations.
- 4. No. Schwelger 2FR Cavity wall bat boxes, 100x70mm prestressed concrete filled over.
- 5. Isoblock single bat brick installed at apex & hole cut into blockwork inner leaf to allow access.
- 6. Exterior leaf heritage bricks to match local vernacular. Reclaimed bricks to be used where possible, subject to approval by the Engineer. Blockwork cavity walls.
- 7. Insulate DPC at 150mm above GL, with engineering bricks below.
- 8. Trenchfill foundation, refer to Structural Notes.
- 9. Lay gas protection membrane conforming with BS 8445:2015, BS 8102:2009, and CIRIA C748. Membrane is to be level to top of foundations and led into foundations.

Legend:

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100% guaranteed site, with 20% and 80% of the site, distributed, only as a means to report on other site, or other purposes.	Client	Approved	Final AEC

19. Walls to be constructed in 100 thick dense aggregate concrete blockwork to BS EN 771-3 with a minimum 100mm gap between courses. All walls to be constructed using class M6 mortar to BS EN 998-2. Piers to be formed into main masonry panel or proprietary austenitic stainless steel starter ties used.
20. All walls to be constructed in 100 thick dense aggregate concrete blockwork to BS EN 771-3 with a minimum 100mm gap between courses. All walls to be constructed using class M6 mortar to BS EN 998-2. Piers to be formed into main masonry panel or proprietary austenitic stainless steel starter ties used.
21. Wall tie spacing to achieve a minimum density of 2.5/m². Ties should be set out in a staggered pattern at a long with a minimum embedment of 50 per leaf.
22. Providing ties within 22% of the vertical edge and 50 there in at least 1 tie for each 300 height of the opening. See recommendations. All ties to have a minimum end bearing of 150. 100 x 70 ties to be F100 by Naylor or approved equivalent. 100x70 ties to be F50 by Naylor or approved equivalent.
23. All masonry workmanship to be carried out in accordance with BS 8000-3:2001.
24. Trenchfill foundations to cavity wall to be minimum 450 wide and bear at 1200 below ground level. Trenchfill formation of foundation excavations are to be finished with a minimum 50mm GEM concrete to BS 6800. The design assumes a compact gravel/sand as defined in BS 6103-1 will be encountered at a 1.5m bgl. The Contractor is to verify the ground conditions prior to commencement of the construction through a site trial pit and to provide a report to the Engineer. The design assumes a compact gravel/sand as defined in BS 6103-1 will be encountered at a 1.5m bgl. All future planning should be completed in accordance with NHBC Standards, chapter 4.2 - Building Near Tress.



1 1 : 50

2 1 : 50

Notes for Longitudinal Section:

- 1. Brickwork exterior wall, heritage bricks to match local vernacular. Reclaimed bricks to be used where possible, subject to approval by the Engineer. With a minimum 100mm gap between bricks. Exterior walls to Cool Room to have 50mm insulation and 18mm WBP plywood boarding.
- 2. Venequeer GX DPC at 150mm above GL, ensure continuous across cavity, foundation level.
- 3. Lay gas protection membrane conforming with BS 8445:2015, BS 8102:2009, and CIRIA C748. Membrane is to be level to top of foundations and led into foundations.
- 4. No. Schwelger 2FR Cavity wall bat boxes, 100x70mm prestressed concrete filled over.
- 5. Isoblock single bat brick installed at apex & hole cut into blockwork inner leaf to allow access.
- 6. Exterior leaf heritage bricks to match local vernacular. Reclaimed bricks to be used where possible, subject to approval by the Engineer. Blockwork cavity walls.
- 7. Insulate DPC at 150mm above GL, with engineering bricks below.
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- 9. Lay gas protection membrane conforming with BS 8445:2015, BS 8102:2009, and CIRIA C748. Membrane is to be level to top of foundations and led into foundations.

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100% guaranteed site, with 20% and 80% of the site, distributed, only as a means to report on other site, or other purposes.	Client	Approved	Final AEC

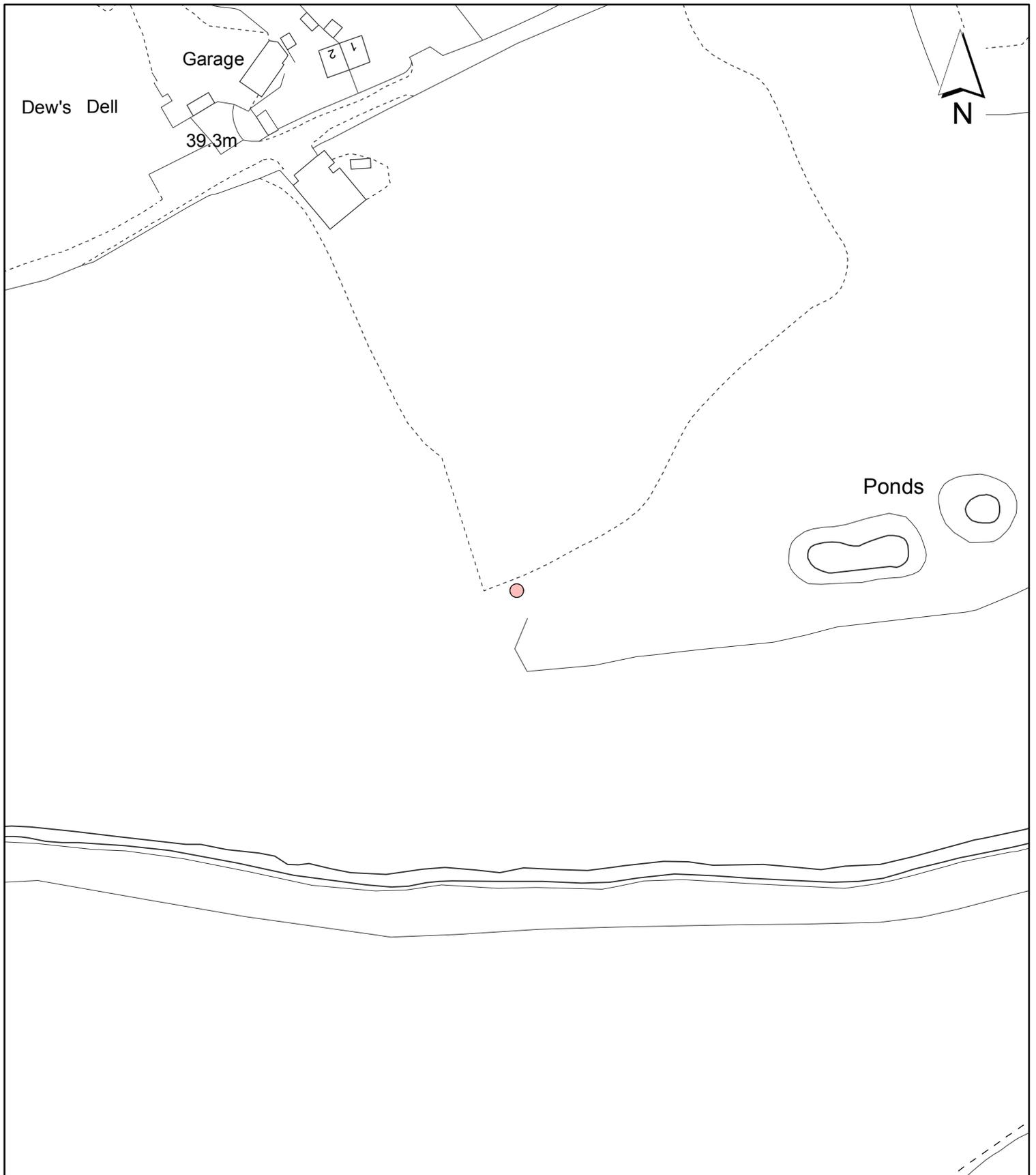
Zone	COLE VALLEY VIADUCT & SURFACE WORKS	Project/Contract	ENABLING WORKS CENTRAL - EWP1-2103
Design Title	OUTLINE DESIGN	Designer/Author	ENVIRONMENTAL
Drawn By	SL	Checked	Approved
Date	22/06/20	Scale	Misc
Stability Code	S4	Fit for Stage Approval	Fit
Drawn No.	FEWB-FUS-EV-DCA-CST1_CLO1-01-0304	Rev	P02

Drawn By	SL	Checked	Approved
Date	22/06/20	Scale	Misc
Stability Code	S4	Fit for Stage Approval	Fit
Drawn No.	FEWB-FUS-EV-DCA-CST1_CLO1-01-0304	Rev	P02

Drawn By	SL	Checked	Approved
Date	22/06/20	Scale	Misc
Stability Code	S4	Fit for Stage Approval	Fit
Drawn No.	FEWB-FUS-EV-DCA-CST1_CLO1-01-0304	Rev	P02

Drawn By	SL	Checked	Approved
Date	22/06/20	Scale	Misc
Stability Code	S4	Fit for Stage Approval	Fit
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Drawn By	SL	Checked	Approved
Date	22/06/20	Scale	Misc
Stability Code	S4	Fit for Stage Approval	Fit
Drawn No.	FEWB-FUS-EV-DCA-CST1_CLO1-01-0304	Rev	P02



Notes:

 Site boundary

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Site Address:

**ECOLOGICAL MITIGATION SITE
 HARVIL ROAD**

Planning Application Ref:

73195/APP/2020/1946

Scale:

1:1,250

Planning Committee:

HS2 Page 42

Date:

October 2020

**LONDON BOROUGH
 OF HILLINGDON
 Residents Services
 Planning Section**

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 01895 250111



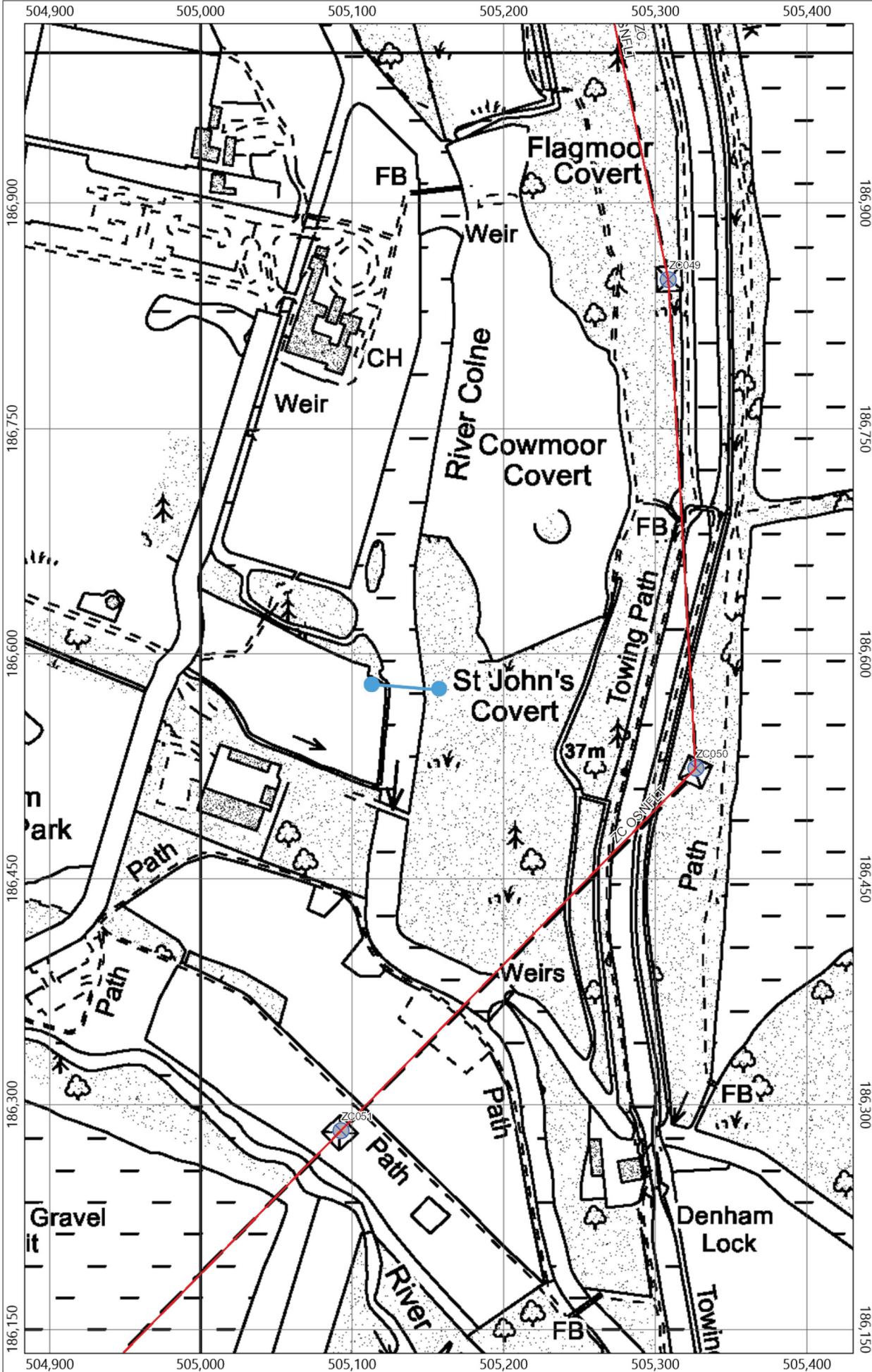
HILLINGDON
 LONDON

Item No. **Report of the Head of Planning and Enforcement**

Address: BETWEEN RIVER COLNE & G.U.CANAL NORTH OF DENHAM LOCK
UXBRIDGE

Development: Request for approval of 'bringing scheduled works and depots into use'
(Paragraph 9) under Condition imposed by Schedule 17 to the High Speed
Rail (London - West Midlands) Act 2017 relating to the diversion of the
National Grid ZC overhead power line in the Colne Valley .(Schedule 1 Work
No. 2/4B) -a temporary bridge over the River Colne).

LBH Ref Nos: 42471/APP/2020/2631



Legend:

- OHL 400Kv Commissioned
- OHL 275Kv Commissioned
- OHL 132Kv & Below Commissioned
- Towers Commissioned
- Tower Wireframe
- Commissioned
- Decommissioned Group
- Planned and Spares

Notes:

Bridge location shown by blue line



Scale: 1: 2,500

Print by: Smith, Aileen

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Date: 16/06/2020
Time: 16:50:14

Page size: A3 Portrait

Page 44
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Note: Any sketches on the map are approximate and not captured to any particular level of precision.



Consolidated Land Boundary (Phase 1 H

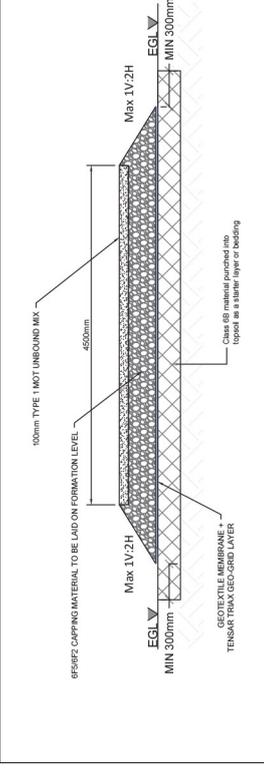
Borough / District / Unitary boundaries



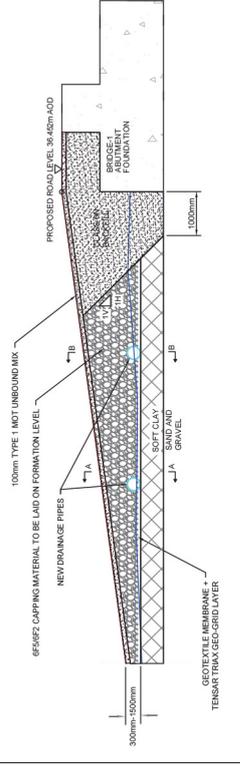
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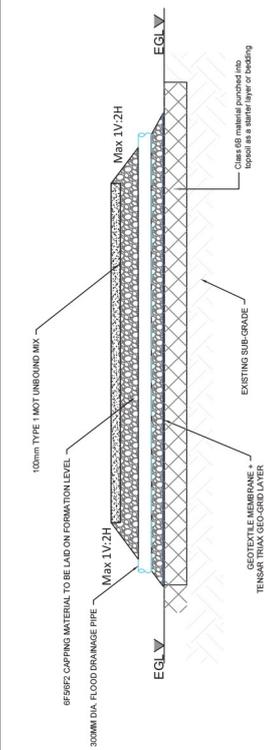
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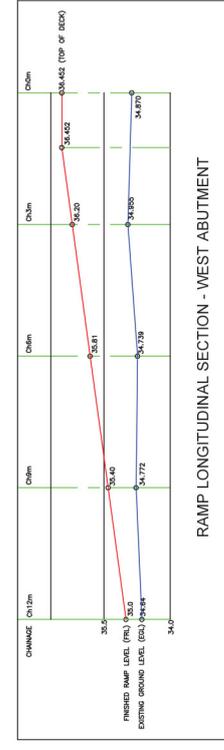
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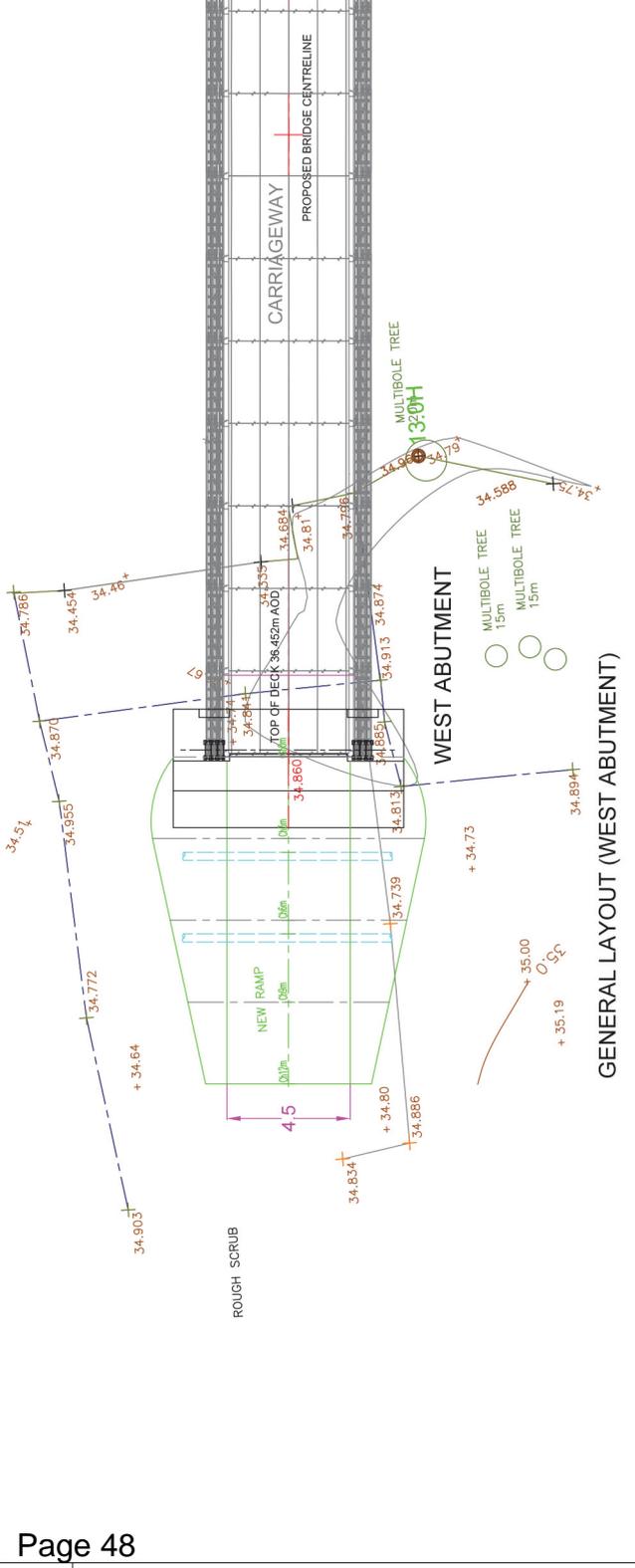
SECTION THROUGH BRIDGE-1 RAMP WEST ABUTMENT APPROACH



CROSS SECTION B-B



RAMP LONGITUDINAL SECTION - WEST ABUTMENT



GENERAL LAYOUT (WEST ABUTMENT)

- NOTES:**
1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED.
 2. SETTING OUT OF ALL WORKS TO BE AGREED ON SITE WITH OVERSEEING ORGANISATION.
 3. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DESIGN DRAWINGS AND SPECIFICATIONS.
 4. ALL LEVELS ARE IN METERS.
 5. CULVERT PIPE DIAMETER AND QUANTITY ARE INDICATIVE. SUBJECT TO CONFIRMATION UPON COMPLETION OF DETAILED DESIGN.
 6. A 'V' SHAPE LINEAR DRAINAGE CHANNEL TO BE CONSTRUCTED AT THE SLOPE SIDE OF THE RAMP, 150MM BELOW FORMATION LEVEL. DRAINAGE CHANNEL TO HAVE A GRADIENT OF 2% TO 6% TO ENSURE WATER FLOW AND PREVENT SILT ACCUMULATION.
 7. THE GRANULAR MATERIAL MUST BE CLEAN, DURABLE, FREE DRAINING AND WELL GRADED, WITH LESS THAN 15% FINE BY WEIGHT.
 8. THE TIME BETWEEN EXPOSING THE SUBGRADE AND INSTALLATION OF THE RAMP SHOULD BE MINIMUM 24 HOURS.
 9. REGULAR VIBRO CONTROL SHOULD BE UNDERTAKEN DURING PLACEMENT AND COMPACTION OF THE GRANULAR MATERIAL. COMPACTION TO COMPLY WITH THE METHOD IN TABLE 8/5 OF SPECIFICATION FOR HIGHWAYS SERIES 800 - EARTHWORKS.

Babcock International Group
 www.babcockinternational.com

nationalgrid
 National Grid Plc, Warwick Technology Park, Gilvies Hill, Warwick, CV34 6DA

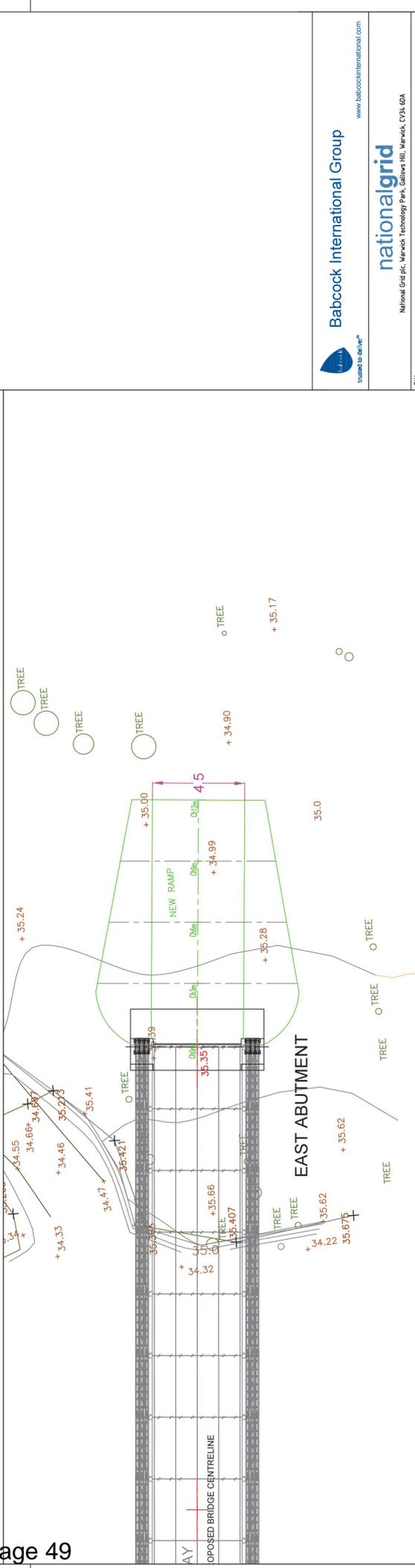
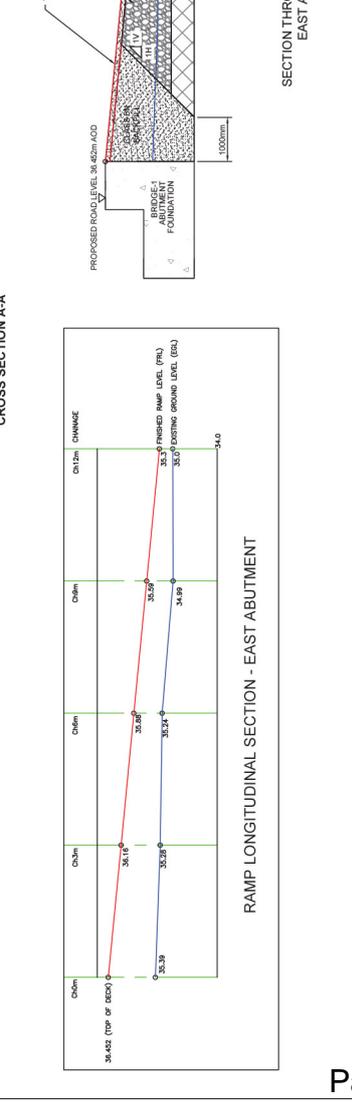
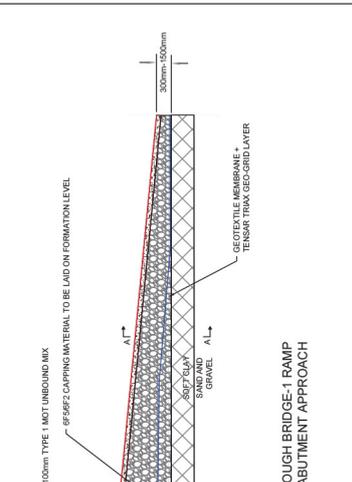
Title
 275KV OVERHEAD TRANSMISSION LINE
 GENERAL LAYOUT & CROSS SECTIONAL VIEW
 OF WEST ABUTMENT & RAMP

Site	ZC ROUTE	A2
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NG Drawing Number	51350/3913	ISSUE
Sheet No.	1	No. of Sheets
SCALE	1:3000	A

ISSUE	A	NG Investment No.	32653
DRAWN	PWH	Remarks	FIRST ISSUE
CHECKED	GG	DATE	29/06/2020
APPROVED	HN		

0 100 200

- NOTES:**
1. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED.
 2. SETTING OUT OF ALL WORKS TO BE AGREED ON SITE WITH OVERSEEING ORGANISATION.
 3. THIS DRAWING IS TO BE READ IN CONJUNCTION WITH ALL OTHER RELEVANT DESIGN DRAWINGS AND SPECIFICATIONS.
 4. ALL LEVELS ARE IN METERS.
 5. CULVERT PIPE DIAMETER AND QUANTITY ARE INDICATIVE. SUBJECT TO CONFIRMATION UPON COMPLETION OF DETAILED DESIGN.
 6. A V SHAPE LINEAR DRAINAGE CHANNEL TO BE CONSTRUCTED AT THE SLOPE SIDE OF THE RAMP, 150MM BELOW FORMATION LEVEL. DRAINAGE CHANNEL TO HAVE A GRADIENT OF 2% TO 6% TO ENSURE WATER FLOW AND PREVENT SILT ACCUMULATION.
 7. THE GRANULAR MATERIAL MUST BE CLEAN, DURABLE, FREE DRAINING AND WELL GRADED, WITH LESS THAN 15% FINE BY WEIGHT.
 8. THE TIME BETWEEN EXPOSING THE SUBGRADE AND INSTALLATION OF THE RAMP SHOULD BE MINIMUM 24 HOURS.
 9. ADEQUATE MEASUREMENT CONTROL SHOULD BE UNDERTAKEN DURING PLACEMENT AND COMPACTION OF THE GRANULAR MATERIAL. COMPACTION TO COMPLY WITH THE METHOD IN TABLE 6/5 OF SPECIFICATION FOR HIGHWAYS SERIES 800 - EARTHWORKS.

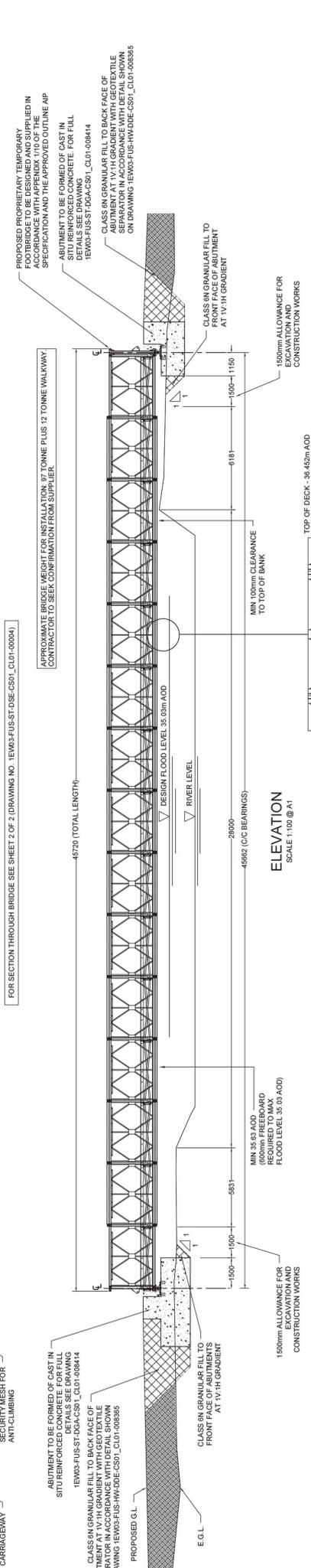
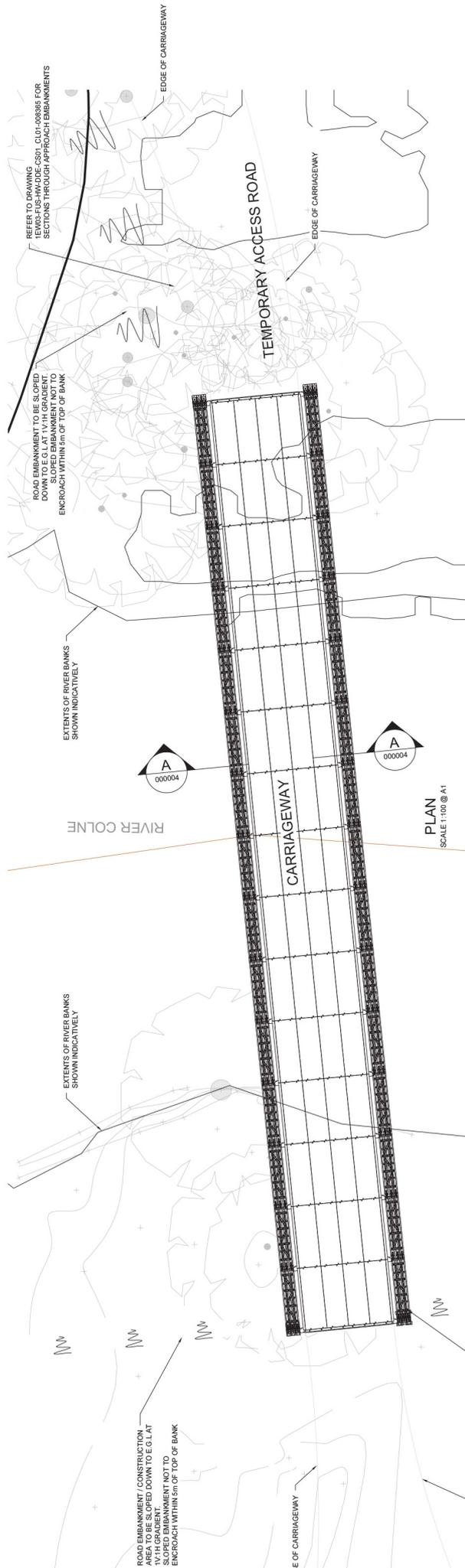


ISSUE		A	NG Investment No.	32653
DRAWN		PWH	Remarks	FIRST ISSUE
CHECKED		GG		
APPROVED		HN	DATE	29/06/2020
Site		ZC ROUTE ZC COLINE VALLEY HS2 DIVERSION		
Originator Drawing Number		51350/3913		
NG Drawing Number		---		
Sheet No.		2		
No. of Sheets		2		
ISSUE		A		
SCALE		1:3000		

Babcock International Group
www.babcockinternational.com

nationalgrid
 National Grid Plc, Warwick Technology Park, Colville Hill, Warwick, CV34 6DA

**275kV OVERHEAD TRANSMISSION LINE
 GENERAL LAYOUT & CROSS SECTIONAL VIEW
 OF EAST ABUTMENT & RAMP**



THIS DRAWING HAS BEEN DEVELOPED BASED ON THE DESIGN PROVIDED BY THE BRIDGE SUPPLIER AND SHALL NOT BE USED FOR CONSTRUCTION.

THE DESIGN OF THE SUBSTRUCTURE WILL CHANGE ONCE THE BRIDGE SUPPLIER HAS UNDERTAKEN THEIR DETAILED DESIGN AND PROVIDED INSTALLATION LOADINGS. DETAILED DESIGN OF THE SUBSTRUCTURE CAN THEN BE COMPLETED AND SUBSEQUENT FOR CONSTRUCTION DRAWINGS SHALL BE PRODUCED.

REV	DESCRIPTION	DN	CHK	APP	DATE
01	DRAFT - WORK IN PROGRESS	AL	AR	AK	01/08/20

REVISIONS	

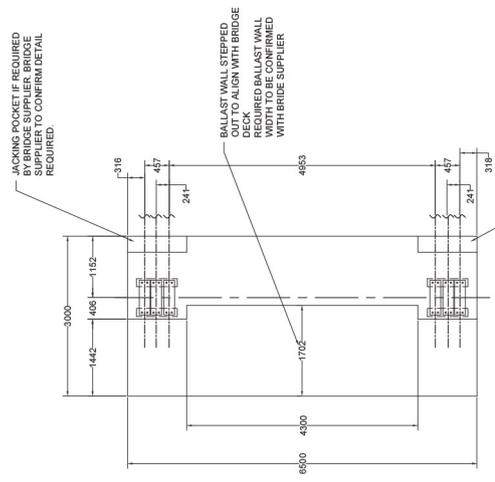
Notes:
1. GROUND WATER LEVELS ARE ANTICIPATED TO BE ABOVE ABUTMENT FORMATION LEVEL AND DEWATERING WILL BE REQUIRED DURING CONSTRUCTION OF ABUTMENTS. DEWATERING TO BE UNDERTAKEN BY OTHERS.
2. THIS DRAWING IS NOT AN ADVISORY DRAWING. TEMPORARY WORKS REQUIREMENTS TO BE ASSESSED BY THE CONTRACTOR.
3. SEE DRAWING 1EW03-FUS-ST-DGA-CS01_CL01-008365 AND ASSOCIATED SERIES 800 DRAWINGS FOR EARTHWORKS REQUIREMENTS.
4. BACKFILL TO THE STRUCTURE IS TO BE CLASS IN MATERIAL MEETING THE REQUIREMENTS OF SPECIFICATION APPENDIX 91 AND COMPACTED IN ACCORDANCE WITH DETAIL SHOWN ON DRAWING 1EW03-FUS-HW00E-CS01_CL01-008365.
5. BACKFILL TO BE BENCH-MARKED INTO EXISTING IN ACCORDANCE WITH SPECIFICATION APPENDIX 64.
6. CONTRACTOR TO IMPLEMENT 5mph SPEED LIMIT ON BRIDGE.
7. INSTALLATED DESIGN BASED ON MAKE PROPRIETARY PRODUCTS. CONTRACTOR MAY USE SIMILAR APPROVED PRODUCTS SUBJECT TO APPROVAL WITH THE BRIDGE SUPPLIER.
8. MINIMUM CARRIAGEWAY WIDTH TO ACCOMMODATE LIEBHERR MOBILE CRANE (TM1200-5.1 INCLUDING SWEEP PATH).
9. ALL DIMENSIONS IN mm UNLESS OTHERWISE STATED.
10. FINISHED GROUND LEVELS TO BE CONFIRMED AT DETAILED DESIGN STAGE.

Pell Frischmann	
MULLIS HEDGE COLNEMAN, COLNEMAN ROAD, STONINGHAM, SOUTHAMPTON, HANTS, RG20 0JN Telephone: +44 (0)1703 207 244 Fax: +44 (0)1703 278 374 Email: pell@pellfrischmann.com	
Project	BUCKS GOLF COURSE
Client	BGC TEMPORARY BRIDGE NO. 1 RR RIVER COLNE GENERAL ARRANGEMENT
Subsidiary	BUCKS GOLF COURSE
Scale	As Shown @ A1
Drawing No.	DK103875107-H-001
Revision	01



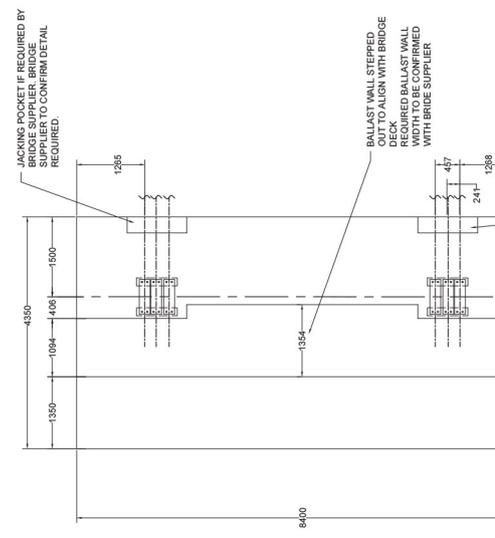
THIS DRAWING HAS BEEN DEVELOPED BASED ON PRELIMINARY LOADS PROVIDED BY THE BRIDGE SUPPLIER AND SHALL NOT BE USED FOR CONSTRUCTION.

THE DESIGN OF THE SUBSTRUCTURE WILL CHANGE ONCE THE BRIDGE SUPPLIER HAS UNDERTAKEN THEIR DESIGN REQUIREMENTS AND FINAL COMPLETED LOADINGS. DETAILED DESIGN OF THE SUBSTRUCTURE CAN THEN BE COMPLETED AND SUBSEQUENT FOR CONSTRUCTION DRAWINGS SHALL BE PRODUCED.



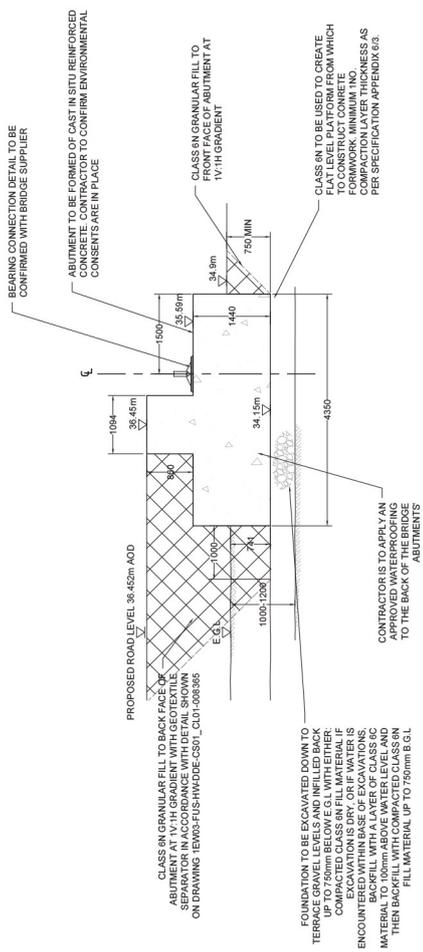
East Abutment Plan
Scale 1:50 @ A1

BEARING ARRANGEMENT SHOWN INDICATIVELY. TO BE CONFIRMED WITH BRIDGE SUPPLIER AT DETAILED DESIGN.

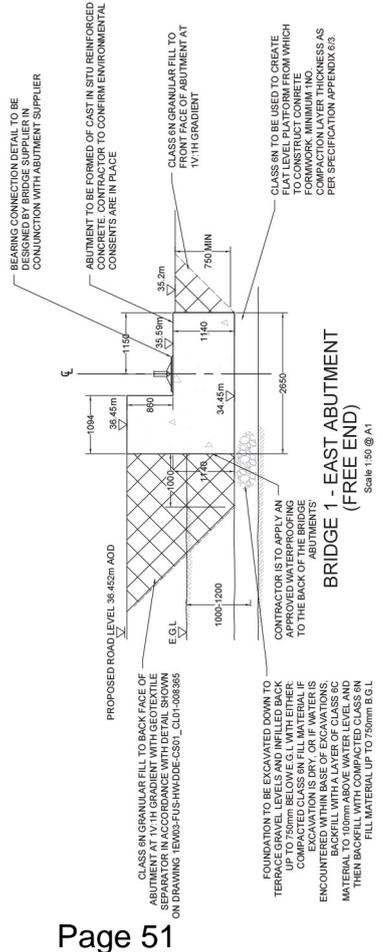


West Abutment Plan
Scale 1:50 @ A1

BEARING ARRANGEMENT SHOWN INDICATIVELY. TO BE CONFIRMED WITH BRIDGE SUPPLIER AT DETAILED DESIGN.



BRIDGE 1 - WEST ABUTMENT (FIXED END)
Scale 1:50 @ A1



BRIDGE 1 - EAST ABUTMENT (FREE END)
Scale 1:50 @ A1

SELF WEIGHT OF SUBSTRUCTURE TO BE CONFIRMED BY BRIDGE SUPPLIER. ABUTMENT TO BE DESIGNED ACCORDINGLY.

REV	DESCRIPTION	DATE	APP	CHK	DATE
01	DRAFT - WORK IN PROGRESS		AL	AR	01/08/20

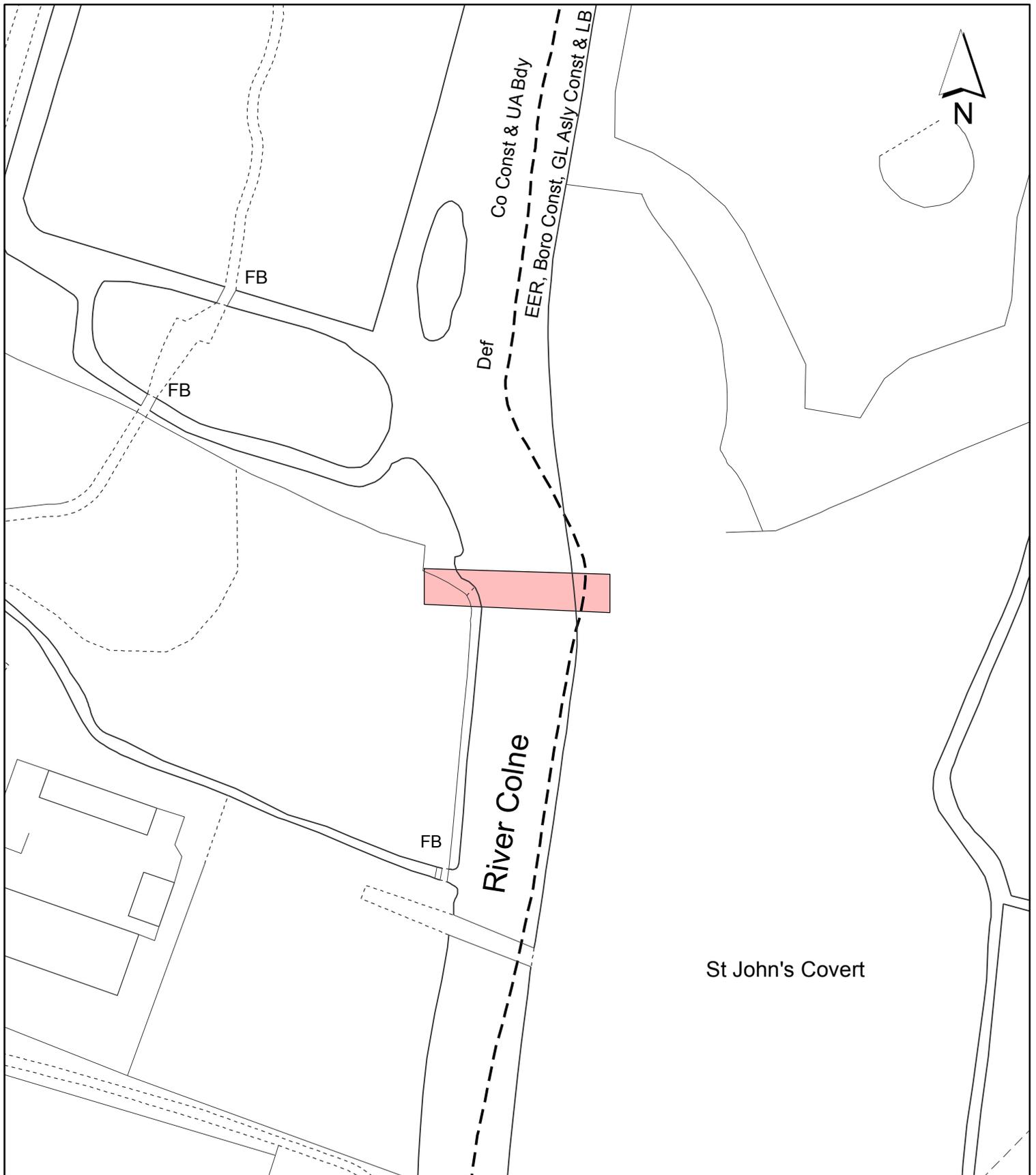
NAME	DATE
Drawn: AL	01/08/20
Checked: AR	01/08/20
Approved: AR	01/08/20

PROJECT	DRAWING TITLE	SUBSTRY	SCALE	DATE
BUCKS GOLF COURSE	BGC TEMPORARY BRIDGE NO. 1 RR RIVER COLNE ABUTMENT DETAILS	DR007 - WORK IN PROGRESS	As Shown @ A1	01/08/20

COMPANY	PROJECT	DRAWING NO.	REVISION
Pell Frischmann	BGC TEMPORARY BRIDGE NO. 1 RR RIVER COLNE ABUTMENT DETAILS	DK103875107-H-002	01

NOTES:

- USE WALL STABILITY WHEN EXCAVATING THE FOUNDATIONS IS NOT KNOWN. TEMPORARY WORKS REQUIREMENTS TO BE ASSESSED BY THE CONTRACTORS.
- BACKFILL TO THE STRUCTURE IS TO BE CLASS B6 MATERIAL MEETING THE REQUIREMENTS OF SPECIFICATION APPENDIX 6.0 AND COMPACTED IN ACCORDANCE WITH SPECIFICATION FOR HIGHWAY WORKS TABLE 6.0 AND CLAUSE 6.2. MAXIMUM BACKFILL GRADIENT TO BE 1V:1H.
- FOUNDATION TO BE EXCAVATED DOWN TO TERRACE GRAVEL LEVELS AND INFILLED BACK WITH CLASS B6 GRANULAR FILL MATERIAL IF EXCAVATION IS DRY, OR IF WATER IS ENCOUNTERED WITHIN BASE OF EXCAVATIONS, BACKFILL WITH A LAYER OF CLASS B6 MATERIAL TO 100mm ABOVE WATER LEVEL AND THEN BACKFILL WITH COMPACTED CLASS B6 FILL MATERIAL UP TO 750mm B.G.L.
- FOUNDATION TO BE EXCAVATED DOWN TO TERRACE GRAVEL LEVELS AND INFILLED BACK WITH CLASS B6 GRANULAR FILL MATERIAL IF EXCAVATION IS DRY, OR IF WATER IS ENCOUNTERED WITHIN BASE OF EXCAVATIONS, BACKFILL WITH A LAYER OF CLASS B6 MATERIAL TO 100mm ABOVE WATER LEVEL AND THEN BACKFILL WITH COMPACTED CLASS B6 FILL MATERIAL UP TO 750mm B.G.L.
- FINISHED GROUND LEVELS TO BE CONFIRMED AT DETAILED DESIGN STAGE.



Notes:

 Site boundary

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Site Address:

**BETWEEN RIVER COLNE
 & G.U.CANAL NORTH OF
 DENHAM LOCK**

**LONDON BOROUGH
 OF HILLINGDON**
 Residents Services
 Planning Section

Civic Centre, Uxbridge, Middx. UB8 1UW
 Telephone No.: Uxbridge 01895 250111

Planning Application Ref:

42471/APP/2020/2631

Scale:

1:1,250

Planning Committee:

HS2 Page 52

Date:

October 2020



HILLINGDON
 LONDON